

mk

weea

WESTERN EXPANSION AREA

Design Code

WHITEHOUSE SOUTH (AREA 10)

OCTOBER 2023





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VISION AND
PLACEMAKING

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1. Introducing the Document

1.1. Status and Purpose of the Document

1.1.1. This Design Code sets out a specific form of detailed design guidance comprising a set of written and graphic rules to establish the precise two and three dimensional design elements of Whitehouse South, which is within Area 10 of the Milton Keynes Western Expansion Area (MKWEA). The extent of the Design Code area is identified at Figure 2.

1.1.2. The Design Code instructs the user upon the design of physical components within the Design Code area in order to deliver the placemaking vision identified for the MKWEA. This Design Code provides a series of instructions that are:-

- Essentially graphic, supported by written rules that are technical in nature
- Precise in their design considerations
- Based on the design vision in the Masterplan and development brief
- Three dimensional in scope, focused primarily on urban design considerations
- Focused on mandatory design characteristics

1.1.3. The preparation and approval of Design Codes for the MKWEA is a requirement of the adopted Development Framework (2005) and approved Section 106 Agreement attached to Outline Planning Permission 05/00291/MKPCO (and subsequently renewed by Outline Planning Permission 11/01685/MKPCO).

1.1.4. Aside from meeting the specific requirements of the Outline Planning Permission, the purpose of this document is to communicate the design process and development principles associated with the site masterplan, whilst demonstrating how this part of the site can be developed in accordance with the Outline Planning Permission.

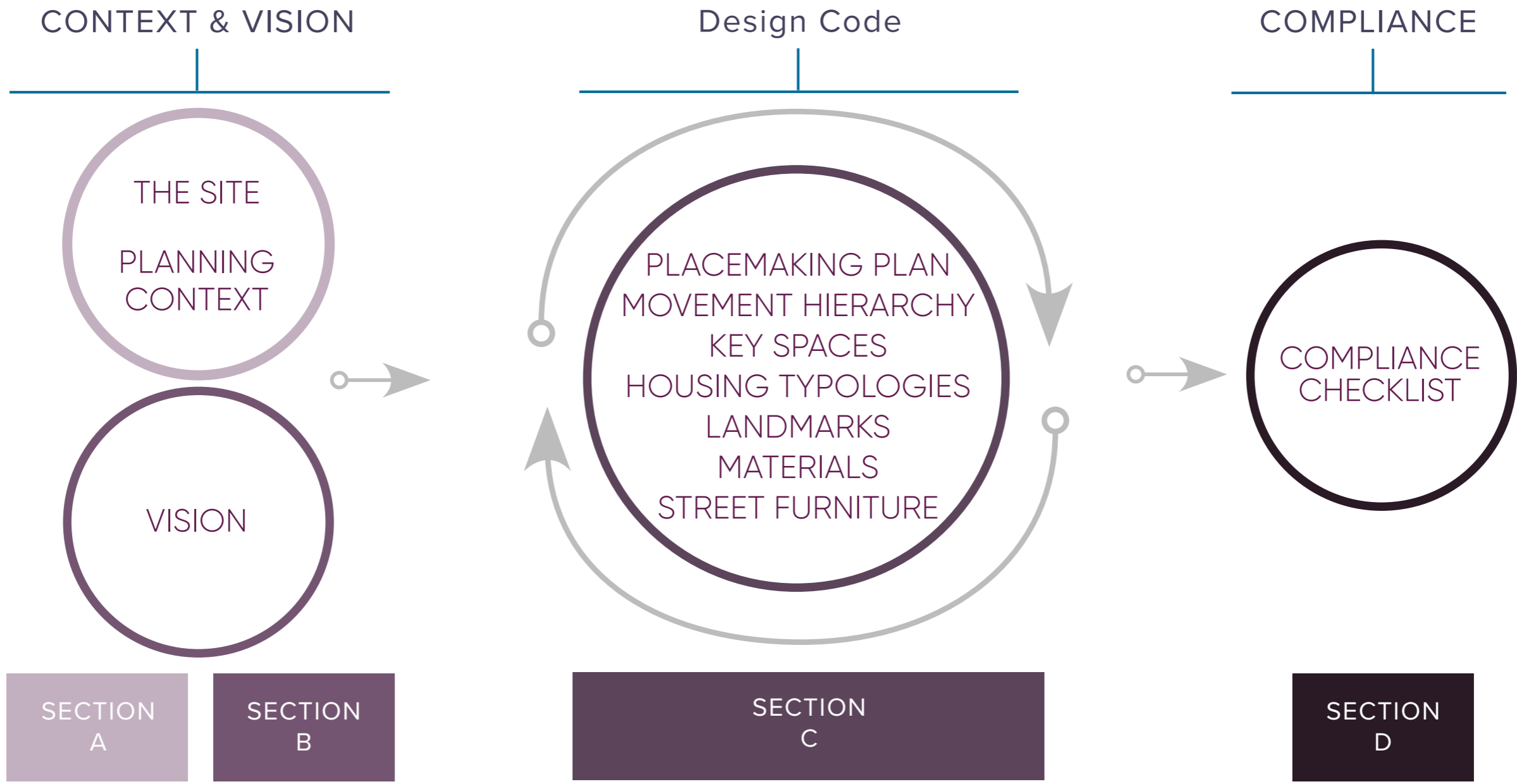
1.1.5. The document explains how a high quality development can be achieved in terms of development use, amount, layout, scale, landscape, appearance, access to the site and the relationship with its surroundings.

1.1.6. This document forms one of a set of Design Codes that are either adopted or planned for Area 10. Of particular importance, the Area 10 Movement Design Code was adopted in September 2008 which approved movement and highways design details across the whole of Area 10. This included street typologies, acceptable parking arrangements and the network of Redways through the development.

1.1.7. The approved Movement Code (2008) is therefore inextricably linked with this Design Code and the two documents need to therefore be read as one. Any variation from the approved Movement Design Code will need to be fully justified.

1.2. Sequence

1.2.1. The document comprises four logically sequenced sections that explain the formulation of the masterplan and the coded elements together with relevant standards that are required to be applied to final development; the Design Code occupying the main body of the document.



2. The Permitted Proposals

2.1. Background

2.1.1. The Western Expansion Area Development Framework was adopted by Milton Keynes Council (MKC) as Supplementary Planning Guidance (SPG) in June 2005. The purpose of the SPG was to set out a comprehensive approach to the future development of the site in order to ensure an orderly sequence of infrastructure phasing and development.

2.2. Outline Planning Permission

2.2.1. The preparation and approval of Design Codes for the MKWEA is a requirement of the adopted Development Framework (2005) and approved Section 106 Agreement attached to Outline Planning Permission 05/00291/MKPCO (and subsequently renewed by Outline Planning Permission 11/01685/MKPCO), which granted permission for:

“Demolition of all buildings, except Whitehouse Farm House, residential development up to 4,330 units, a range of employment uses including class B1, B2 and B8 uses, erection of a secondary school and 3 primary schools (Class D1), a mixed-use centre (including Class A1, A2, A3, A4 and A5 uses), community uses (Class D1), indoor sports and other leisure facilities (Class D2 uses), burial ground, associated structural landscaping, open space and play areas, associated highways and infrastructure improvements.”

2.2.2. Specific conditions attached to the Outline Planning Permission (shown below) also contain a number of requirements to be addressed by the Design Code.

2.3. Area 10 Movement Design Code

2.3.1. This document forms one of a set of Design Codes that are either adopted or planned for Area 10. Of particular importance, the Area 10 Movement Design Code was adopted in September 2008 which approved movement and highways design details across the whole of Area 10. This included street typologies, acceptable parking arrangements and the network of Redways through the development.

2.3.2. The approved Movement Code (2008) is therefore inextricably linked with this Design Code and the two documents need to therefore be read as one. Any variation from the approved Movement Design Code will need to be fully justified.

Extracts from the Outline Planning Approval - Reference No. 05/00921/MKPCO

Condition	Description
Condition 1	Approval of the details of the siting, design and external appearance of the buildings, the means of access and the landscaping (hereinafter called “the reserved matters”) for each Development Parcel shall be obtained from the Local Planning Authority prior to the commencement of that Development Parcel.
Condition 2	Submissions pursuant to this permission shall be in accordance with the Fairfield Development Brief, reference G.0100-119-1 (May 2006), and Masterplan, reference G.0100-116-3, save for any deviations there from otherwise agreed in writing by the Local Planning Authority.
Condition 5	Prior to the submission of the first reserved matters application on each Phase, the programme for development of that Phase, setting out the anticipated timing for development within that Phase shall be submitted to the Local Planning Authority. Any change in programme for a Phase shall be submitted to the Local Planning Authority within ten days of such change.
Condition 10	No part of the development shall exceed the Maximum Size (for buildings within the specified use class of the Town and Country Planning (Use Classes) Order ¹⁹⁸⁷) as set out in Table 1.
Condition 17	Each reserved matters application (submitted pursuant to condition 1) shall include full details of finished floor levels for each building and finished site levels (for all hard surfaced and landscaped areas) in relation to existing site levels. The development shall thereafter be carried out in accordance with the approved level details.
Condition 20	No development shall take place until an Open Space Specification which includes: the details and specification for all areas of open space to be provided within the development this includes Public Space and Open Space; the timing for laying out the Open Space and if the developer wishes to lay out the Public Open Space, the timing for laying out the Public Open Space; and the long term maintenance arrangements for Open Space and Public Open Space if applicable (including anticipated costs) has been submitted to and approved by the Local Planning Authority. The schemes shall be implemented in accordance with the approved details unless otherwise agreed by the Local Planning Authority.
Condition 21	The landscaping scheme required by Condition 1 above shall be submitted to and approved by the Local Planning Authority for a Development Parcel before any part of the work on that Development Parcel is commenced. The scheme shall take into account the Landscape Masterplan approved as part of this outline permission and shall show the numbers, types and sizes of trees and shrubs to be planted and their location in relation to proposed buildings, roads, footpaths and drains. The planting is to be carried out in accordance with the scheme and shall be carried out in phases to be agreed in writing with the Local Planning Authority. Any trees or shrubs removed, dying, severely damaged or diseased on Open Space within five years of planting shall be replaced in the next planting season with trees or shrubs of such size and species as may be agreed by the by Local Planning Authority.
Condition 22	Prior to development commencing on a Development Parcel a Landscape Protection Action Plan for that development parcel shall be submitted to and approved by the Local Planning Authority. The Plan shall highlight all existing landscape and wildlife conservation features to be protected and shall address measures to be carried out to ensure their physical protection and include detailed drawings showing which trees are to be retained and which trees are proposed to be felled or lopped. None of the trees shown to be retained shall be felled or lopped without prior consent in writing from the Local Planning Authority.
Condition 25	Prior to the commencement of development on a Development Parcel a scheme showing the proposed boundary treatment of that Development Parcel shall be submitted to and approved by the Local Planning Authority. The scheme shall show all the type and height of fences, hedges, walls and other means of enclosure, and these shall be provided in accordance with the approved scheme before residential units are first occupied. The approved fence, hedge or wall shall subsequently be retained.
Condition 29	The reserved matters shall take account of the “Secured by Design” principles and be accompanied by a report which demonstrates the principles of “Secured by Design” which the reserved matters incorporate.

2.4. Related Documentation

2.4.1. In addition to the approved Area 10 (Whitehouse) Masterplan and Highway Design Code (2008), this Design Code should be read alongside the Western Expansion Area (WEA) Development Framework (2005) and Development Brief (approved as part of the Outline Planning Permission in 2005) which have been adopted by Milton Keynes Council for the wider site and provide guidance for the preparation of future Design Codes. This document should also be used in conjunction with the Milton Keynes Council's Residential Design Guide (2012) .

2.5. The Role of Master Developer

2.5.1. L&Q Estates has taken on the role of 'Master Developer' for this project. They are providing a high level of direction and control of the development process, to ensure that this new development delivers the 'vision' as set out in the Development Brief.

2.5.2. The Master Developer retains responsibility for the construction of the framework of the site which includes: off site works to highways, on site infrastructure, primary utilities, drainage and servicing land for schools and the public open spaces. They also facilitate the long-term maintenance areas of the site in complying with the adoption criteria of Milton Keynes Council and the MK Parks Trust.

2.5.3. As part of its infrastructure works programme, the Master Developer will prepare serviced residential land parcels that will be sold to house-builders. The Design Codes will be used throughout the disposal of parcels in order to maintain quality of development throughout the life of the scheme.



FIGURE 1: MKWEA COMPOSITE MASTERPLAN



- LEGEND**
- Expansion Areas 10 & 11
 - Residential Blocks
 - Primary Schools
 - Secondary School
 - Employment
 - Local Centre / High Street
 - Reserved Sites for Planned / Future Community Uses
 - Reserved Sites Proposed for Transfer to MK Community Foundation
 - Pavillion
 - Public Open Space
 - Proposed Structural Planting
 - Burial Ground
 - Playing Fields



3. Introducing the Design Code Area

3.1. The Site

3.1.1. Whitehouse South forms part of the wider Whitehouse Area (Area 10) which essentially provides the eastern extent to the wider MKWEA.

3.1.2. Whitehouse South effectively forms part of a new western edge to Milton Keynes. The site extends to the west towards Stratford Road and Whaddon Common Farm. To the east is the existing built extent of Whitehouse which is separated from the site by a new City Street (Barossa Way). To the north and south of Whitehouse South are retained areas of open agricultural land.

3.1.3. Whitehouse South comprises a gently-sloping plateau facing eastwards which historically has been used primarily for pastoral farmland. It contains a number of existing agricultural fields with hedgerowed boundaries (some of which include mature trees) and a number of small drainage ditches which run eastwards down field boundaries before entering the existing built up area.

3.2. Context

3.2.1. The built-up area of Milton Keynes lies to the east of the site. In addition to the existing built extent of Whitehouse, the districts which comprise the adjacent existing built up areas include Two Mile Ash, Crownhill and Grange Farm. These districts comprise a mixture of uses including residential, light industrial, amenity and education facilities. Further to the north lies the town of Stony Stratford and the districts of Kiln Farm and Fullers Slade. Central Milton Keynes, including the mainline railway station which provides direct train services to London Euston and Birmingham and regional shopping centre, is located within 3kms of the site to the east.

3.2.2. Milton Keynes does not have a uniform or common existing vernacular or palette of materials to which new development can respond. It can therefore for the most part create its own character. Milton Keynes Council sees the city as a 'forward thinking 21st century "New City", and feel that this image should be reflected in its Architecture, which should be of its time, and of Milton Keynes.

3.2.3. By contrast to the inherent modernity of Milton Keynes, to the west and south west of the site, along Calverton Lane, lie the traditional villages of Lower, Middle and Upper Weald, known collectively as Calverton as well as the nearby village of Whaddon.



FIGURE 2: DESIGN CODE AREA



EGEND
□ Design Code Area



3.3. Site Elements and Existing Constraints

3.3.1. The following key technical considerations have been addressed by the approved masterplan:

Topography

3.3.2. The design code area gently slopes down from south (112 metres) to north (105 metres). The approved masterplan responds generally to this by ensuring streets run largely parallel or at right angles to the contours.

Important Vegetation

3.3.3. There are a number of important hedgerows and trees within the design code area. These have acted as crucial structuring elements to the masterplan and have assisted in informing the location of streets and open spaces. The overriding principle has been to retain as many of the good quality hedgerows and trees as possible and to locate them within the public realm. They will therefore be highly visible features across the masterplan helping to establish a unique character for the new neighbourhood.

Ecological Features

3.3.4. There are a number of trees within the site which are confirmed or potential bat roosts. A watercourse forms part of the eastern boundary of the site.

3.3.5. Within the wider area a series of ecological enhancement and compensation measures have been delivered including the following:

- 10 New Large Breeding Ponds
- 300 Linear Metres of Hibernaculum
- 22 Refugia Clumps
- Circa 15KM of perimeter newt fencing
- 2 Ha of Native Woodland – 2,400 species planted
- 40 No scrub clumps – 600 species planted
- 700m Linear Native Hedge – 3,000 species planted
- Total 6,000 species planted and complete

Archaeological Features

3.3.6. Watling Street, which runs to the north of the site, is a Roman road and is therefore of important archaeological interest. The southern corner of the site shows evidence of ridge and furrow. There have also been a number of archaeological finds within the site.

Public Rights of Way

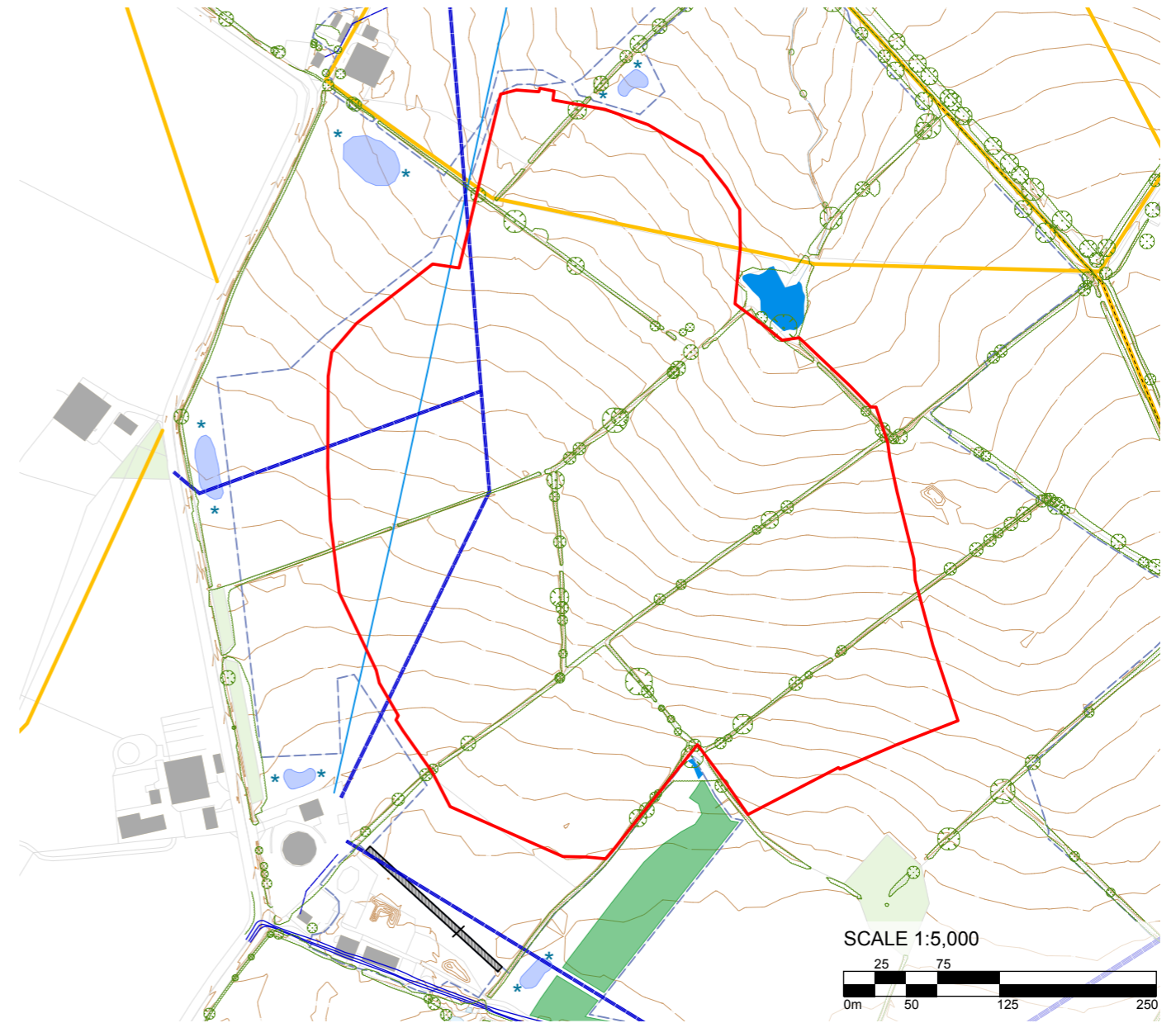
3.3.7. The North Buckinghamshire Way Bridleway (BW023) runs broadly north west to south east close to the north east of the design code area. To the north, a Public Footpath (FP050) runs east to west through the north part of the design code area.



FIGURE 3: SITE CONSTRAINTS



- 0.5m Contours
- Movement**
- Existing PRow
- Utilities**
- Electricity Cable
- Electricity Pole
- Water Main
- Ecology**
- Newt Fencing (0.5m clear corridor on either side)
- Newt Corridor
- Pond & Hibernacular Bunds
- Breeding Pond
- Woodland Areas
- Existing Trees and Hedgerows
- * Refugia



SECTION B /
VISION AND PLACEMAKING

4. Vision

4.1. Vision Context

4.1.1. The vision for the MKWEA has been articulated in several places including the adopted Development Framework (2005) and the Development Brief (2005) approved with the Outline Planning Permission.

4.1.2. The ambition is for a comprehensive extension to the City that creates a sustainable community that, at its heart, is a walkable neighbourhood. The Vision in the Development Brief states that the MKWEA will be “a place where living, working, learning, and leisure are all in close proximity. Amenities, schools, open spaces, cycle networks, and bus stops are all within 5-10 minutes walk” and that “for destinations further afield, there are quality cycle routes and quality bus corridors with good access to the other amenities within Milton Keynes.”

4.1.3. The vision emphasises the need for ‘future proofing’ including flexible buildings (particularly in the local centres) that can change their use over time and reserved sites that can be used in a dynamic way and respond to neighbourhood needs as they emerge. These initiatives acknowledge that the Masterplan is not fixed in time and space.





4.2. A Vision for Whitehouse South

4.2.1. Figure 2 shows the location of Whitehouse South in the context of the approved MKWEA masterplan.

4.2.2. Aside from simply responding to physical constraints and the planning context associated with the site and the Outline Planning Permission, development will be founded on a strong vision of creating a special and distinctive community; a popular and sustainable place to live and relax.

4.2.3. The vision for Whitehouse South is focused on creating a healthy and stimulating 21 Century place to live, learn and grow, it is about catering for who we are and how we live, today and in the future. Drawing on the scale and structure of the wider MKWEA masterplan, development at Whitehouse South will be suburban in scale and provide a comfortable residential density that allows a balanced community to thrive.

4.2.4. Whitehouse South will be characterised by family-orientated residential development within easy access of essential community facilities and set within a network of well-connected, attractive and safe streets and public spaces that encourage walking and cycling and enhance the well-being of its residents.

4.2.5. The architecture of Whitehouse South will be distinctly contemporary but using traditional influences of form and materiality that respects the neighbourhood scale.

SECTION C /
DESIGN CODE

5. Design Code Requirements

5.1. National Policy Requirements

5.1.1. This Design Code sets out a specific form of detailed design guidance to establish key design elements of development proposed at Whitehouse South. The extent of the Design Code area is identified at Figure 2.

5.1.2. The NPPF (July 2021) sets out at paragraph 73(c) that “appropriate tools such as masterplans and design guides or codes are used to secure a variety of well-designed and beautiful homes to meet the needs of different groups in the community”.

5.1.3. NPPF Paragraph 126 states that the creation of high quality, beautiful and sustainable buildings and places is fundamental to what the planning and development process should achieve, and that, good design is a key aspect of sustainable development.

5.1.4. NPPF Paragraphs 132-136 emphasise the importance of design quality through the evolution and assessment of development proposals. They state that development that is not well designed should be refused, especially where it fails to reflect local design policies and government guidance on design, taking into account any local guidance and supplementary planning documents such as design guides and codes.

5.1.5. The NPPF emphasises that all local planning authorities should prepare design guides or codes consistent with the principles set out in the National Design Guide and National Model Design Code, and which reflect local character and design preferences and that design codes and guides should provide a framework for creating high quality places.

5.2. Outline Planning Permission Requirements

5.2.1. In addition to national planning requirements, Conditions and the Section 106 Agreement attached to Outline Planning Permission 05/00291/MKPCO (and subsequently renewed by Outline Planning Permission 11/01685/MKPCO) requires that certain matters are addressed by the Design Code including:

- Density;
- Details of building types;
- Parking provision;
- Building materials;
- Key elevational and architectural principles;
- Location of landmark buildings and gateways;
- Building heights;
- Street types and block principles;
- Roofs and boundaries;
- Public realm materials including carriageways;
- Footways;
- Landscaping;
- Street furniture; and
- Detailing the extent to which or providing guidelines (if appropriate) as to how residential units and/or commercial units can be altered once constructed.

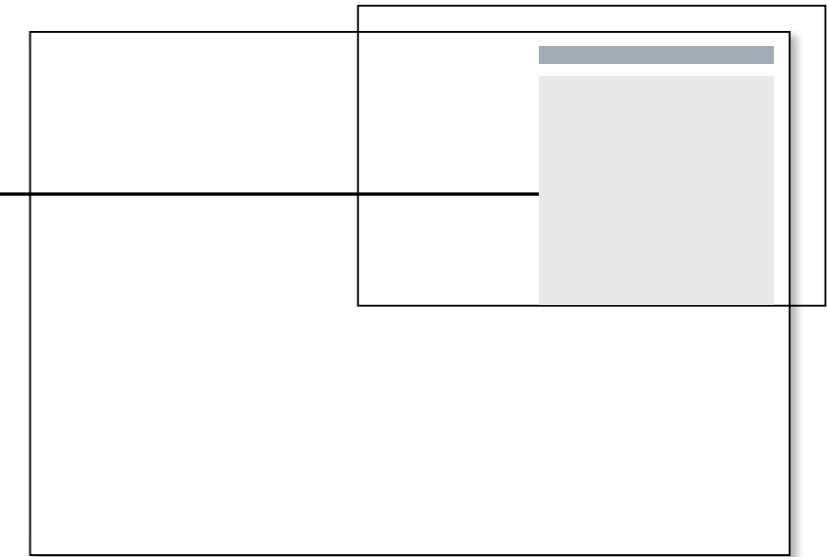
5.2.2. Some of the required elements therefore relate to specific matters of detail whilst others require a site-wide response. The approach of this code is to address a number of these matters strategically, through site-wide plans and others specifically, using Mandatory Principles as part of the overall approach to addressing placemaking. Ultimately, the Design Code sets the general direction of placemaking and architecture to be provided within the coding area which should be modern in its appearance.

5.2.3. The Mandatory Principles relate directly to the specific elements that the Conditions and Section 106 Agreement requires the Design Code to address. The principles are clearly identified within status boxes on the right hand side of each page (see example provided right). Reserved Matters Applications are required to comply with Mandatory Principles in all cases unless a change can be justified (i.e. ‘comply or justify’).

5.3. Area 10 Movement Design Code

5.3.1. As noted above, the Area 10 Movement Design Code, adopted in September 2008, approved movement and highways design details across the whole of Area 10. This included street typologies, acceptable parking arrangements and the network of Redways through the development. The provisions of the approved Movement Design Code are reflected in the information below.

EXAMPLE OF MANDATORY PRINCIPLES BOX



MANDATORY PRINCIPLES

1 Building Form

Detached and semi-detached dwellings irregularly arranged (but with principal elevations facing the edge) to provide an informal response to the adjacent natural character of mature hedgerows.

Driveways and garages to side of properties. Parking to be positioned no further beyond the building line to ensure dwelling elevation is the most dominant.

A 'Mandatory Principles' box is located on the right hand side of every spread of the Design Code containing mandatory coding information.

6. Residential Densities

6.1. Overview

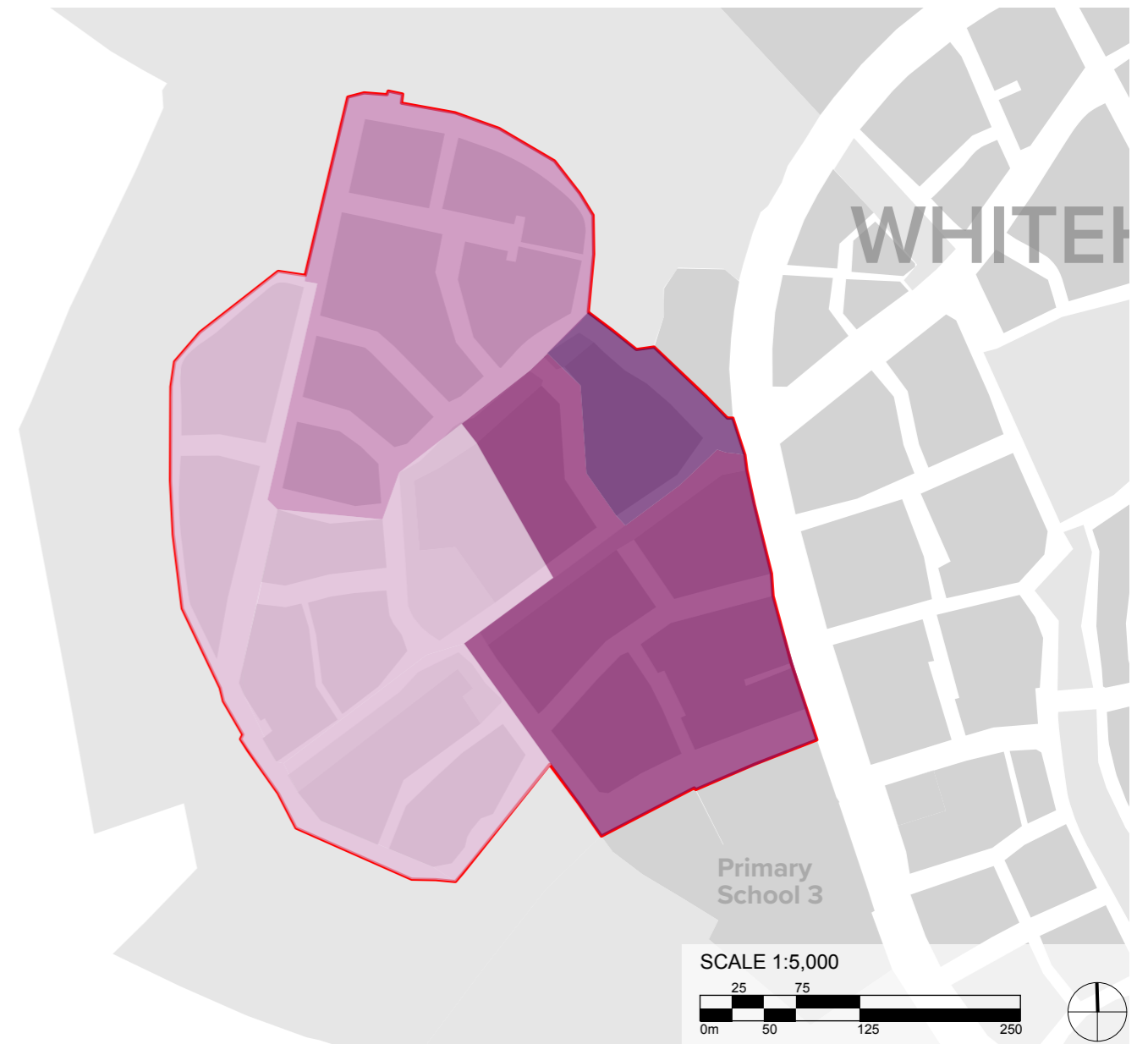
6.1.1. Residential densities across the MKWEA are established by the adopted MKWEA Development Framework (2005) .

6.1.2. In accordance with the approved Development Framework, a range of building densities are to be applied across the entire MKWEA. Higher densities (average 50 dwellings per hectare) will be required adjacent to the City Street, taking advantage of good access to bus services and community facilities. Further away from the City Street, medium densities (average 40 dwellings per hectare) will be required. Towards the edge of the development, lower densities (average 30 dwellings per hectare) will be required to provide a softer interface to the edges of the site.



FIGURE 4: APPROVED DENSITIES
TAKEN FROM THE ADOPTED MKWEA
DEVELOPMENT FRAMEWORK (2005)

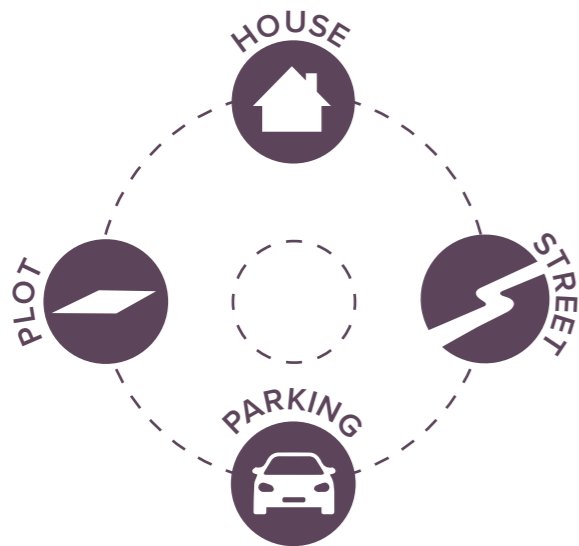
- LEGEND
- Site Boundary
- Densities
- High Density / 50 dph
 - High Density / 40 dph
 - Medium Density / 30-40 dph
 - Low Density / up to 30 dph



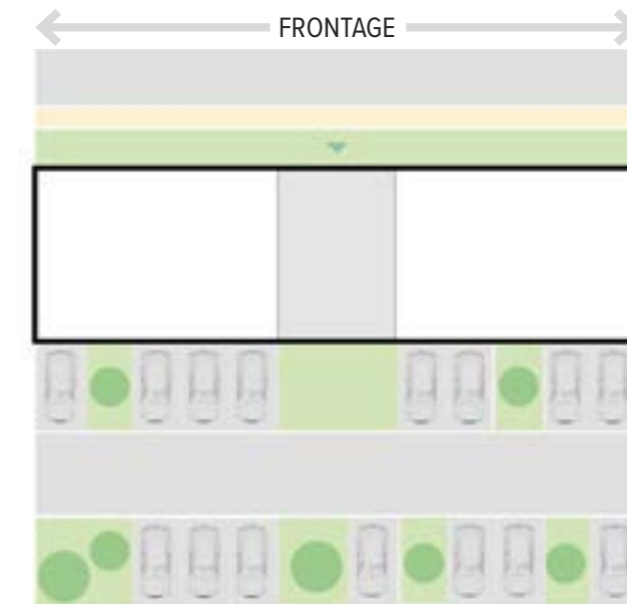
7. Housing Typologies

7.1. Overview

7.1.1. The following information provides mandatory requirements in respect of the relationship between housetypes, street and the plot. The approved Area 10 Movement Code is clearly an important consideration in this respect. The housing typologies shown are not specific to street type; the table (right) shows which streets may support specific housing typologies. The typology diagrams are provided as guidance to enable balanced and valued judgements on wider detailed scheme design.



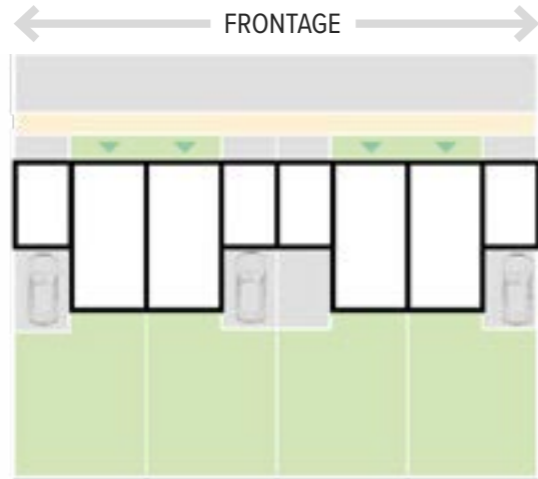
STREET TYPOLOGY	HOUSING TYPOLOGY
City Street	Apartment Buildings, Linked Form (Option 1 & 2)
Link Street	Linked Form (Option 1 & 2), Terraced Houses (Option 1 & 2)
Residential Street	Linked Form (Option 1 & 2), Terraced Houses (Option 1 & 2), Semi-detached Houses, Detached Houses
Edge/Green Street	Linked Form (Option 1 & 2), Terraced Houses (Option 1 & 2), Semi-detached Houses, Detached Houses
Community Street	Linked Form (Option 1 & 2), Terraced Houses (Option 1 & 2), Semi-detached Houses
Mews Street	Linked Form (Option 1 & 2), Terraced Houses (Option 1 & 2), Semi-detached Houses



Rear Courtyard Parking

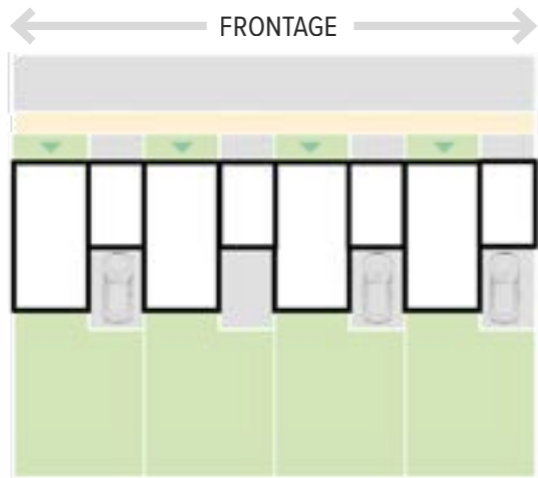
APARTMENT BUILDINGS





Parking to the side under car-ports

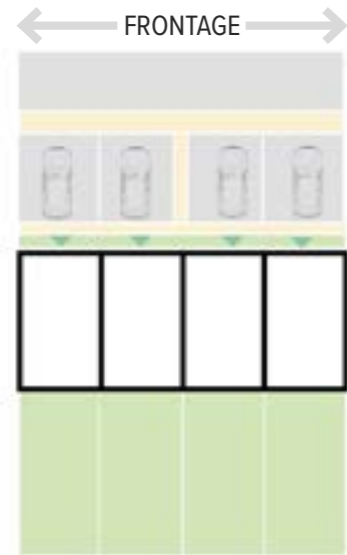
LINKED FORM - OPTION 1



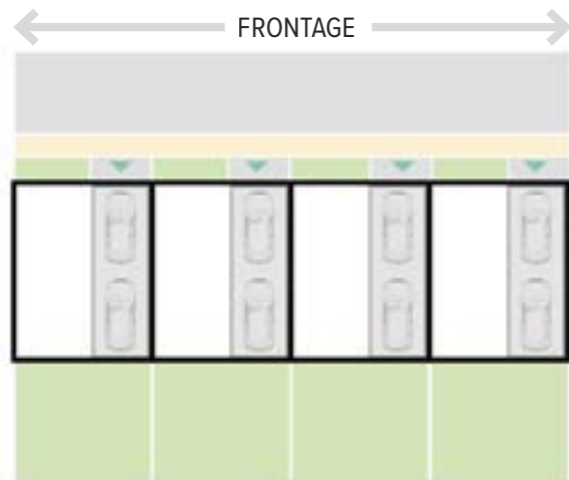
Parking to the side under car-ports

LINKED FORM - OPTION 2



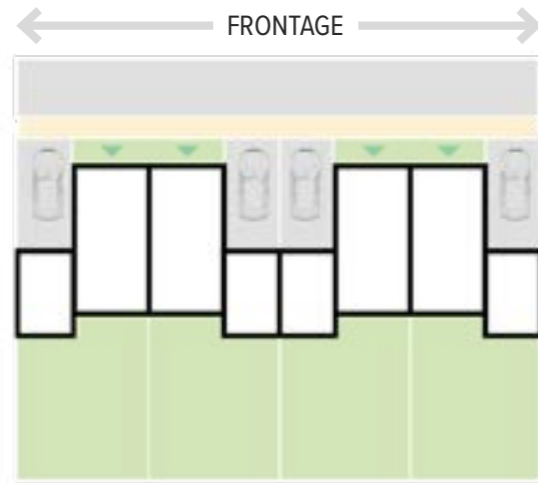


Parking to the Front
 (planted areas to break up hard surfacing / max. 3 spaces in a run)
 TERRACED HOUSES - OPTION 1



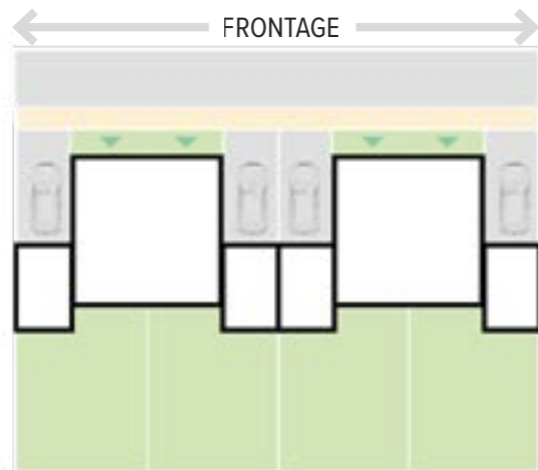
Integral Parking accessed from the front
 TERRACED HOUSES - OPTION 2





Parking to the side

SEMI-DETACHED HOUSES



Parking to the side

DETACHED HOUSES



8. Placemaking Plan

8.1. Placemaking Plan

8.1.1. The Placemaking Regulatory Plan (right) identifies the various placemaking components of the Masterplan that are addressed in detail by the Design Code. Thereafter, corresponding Mandatory Principles are provided, supported by written and graphic material, which can be cross-referred to the Placemaking Regulatory Plan to establish precise design requirements to be taken forward in the design of Reserved Matters proposals for the site.

8.1.2. The Placemaking Regulatory Plan has evolved directly from the Masterplan and identifies the placemaking components of the Masterplan that should be coded so that the Masterplan can be realised.

8.1.3. All information contained within the Placemaking Regulatory Plan must be cross-referred to the Mandatory Principles and thereafter taken forward in the design of Reserved Matters proposals for the site.

8.1.4. The key components of the masterplanning approach which the Mandatory Principles relate to as shown in the Placemaking Regulatory Plan are as follows:

Movement Hierarchy (as approved by the Movement Design Code 2008):

- City Street
- Link Street
- Residential Street
- Edge Street
- Community Street
- Mews Street

Key Spaces:

- District Park
- Local Park
- Whitehouse South Neighbourhood Green
- Whitehouse South Allotments
- Whitehouse South Cemetery

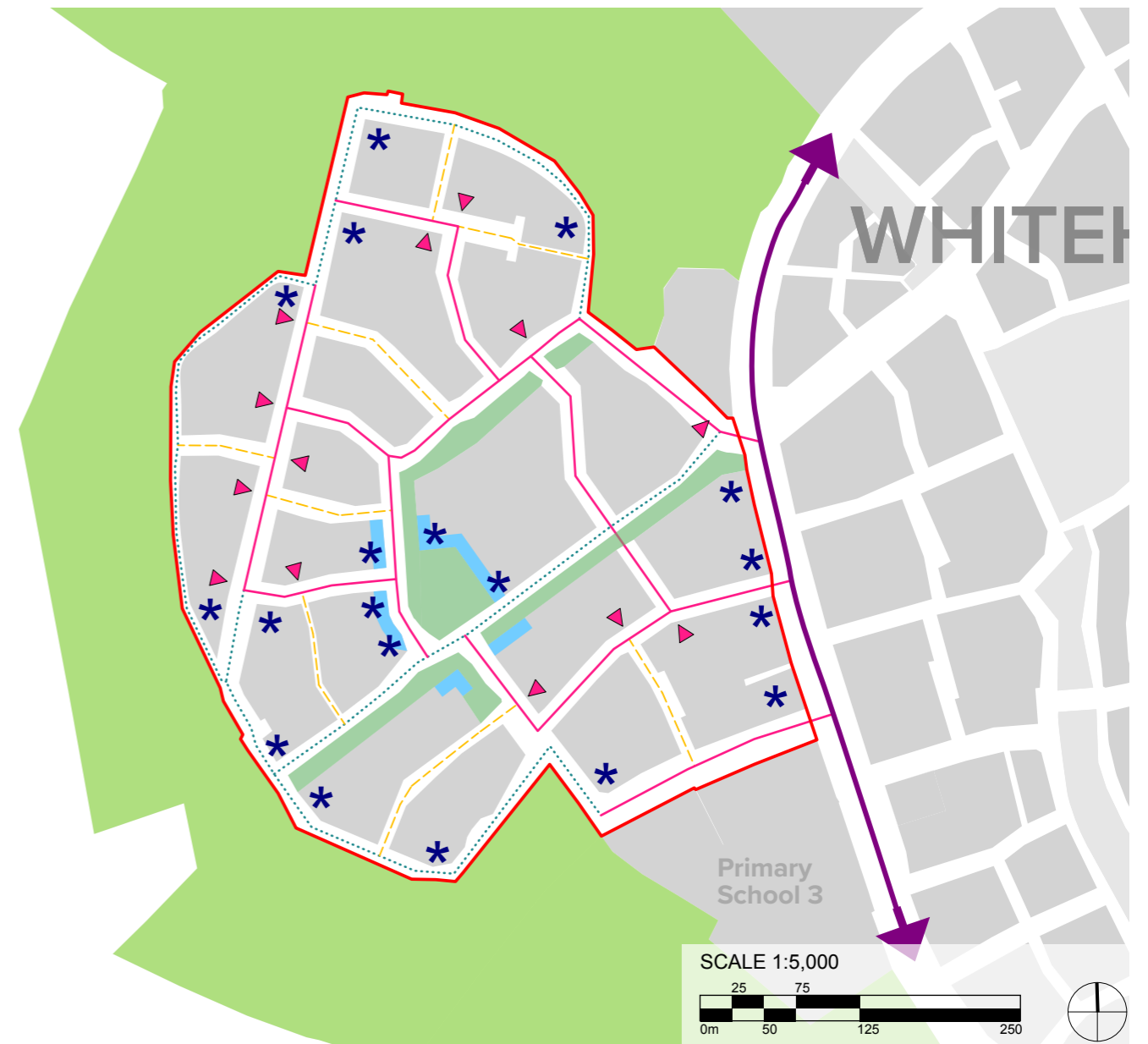
Key Frontages & Landmarks:

- Whitehouse South Primary School
- Key Corners
- Vista Termination



FIGURE 5: PLACEMAKING PLAN

- LEGEND**
- Site Boundary
 - Movement Hierarchy**
 - City Street
 - Residential Street
 - Edge Street
 - Community Street
 - Special Places**
 - Green Corridors
 - Parkland
 - Landmarks & Frontages**
 - * Key Corners
 - ▲ Vista Termination
 - Whitehouse South Green Key Fro



9. Movement Hierarchy (as approved by the MKWEA Movement Design Code 2008)

9.1. Context and Approach

9.1.1. The movement hierarchy in this Design Code follows the precise specifications and details for each street typology as already set out by the approved MKWEA Area 10 Movement Design Code (September 2008).

9.1.2. The street hierarchy has been designed to embody the philosophy and principles encapsulated in the Manual for Streets (2007) and other relevant best practice recommendations. The detailed design of each route will be based on an understanding of the character of the areas through which it passes.

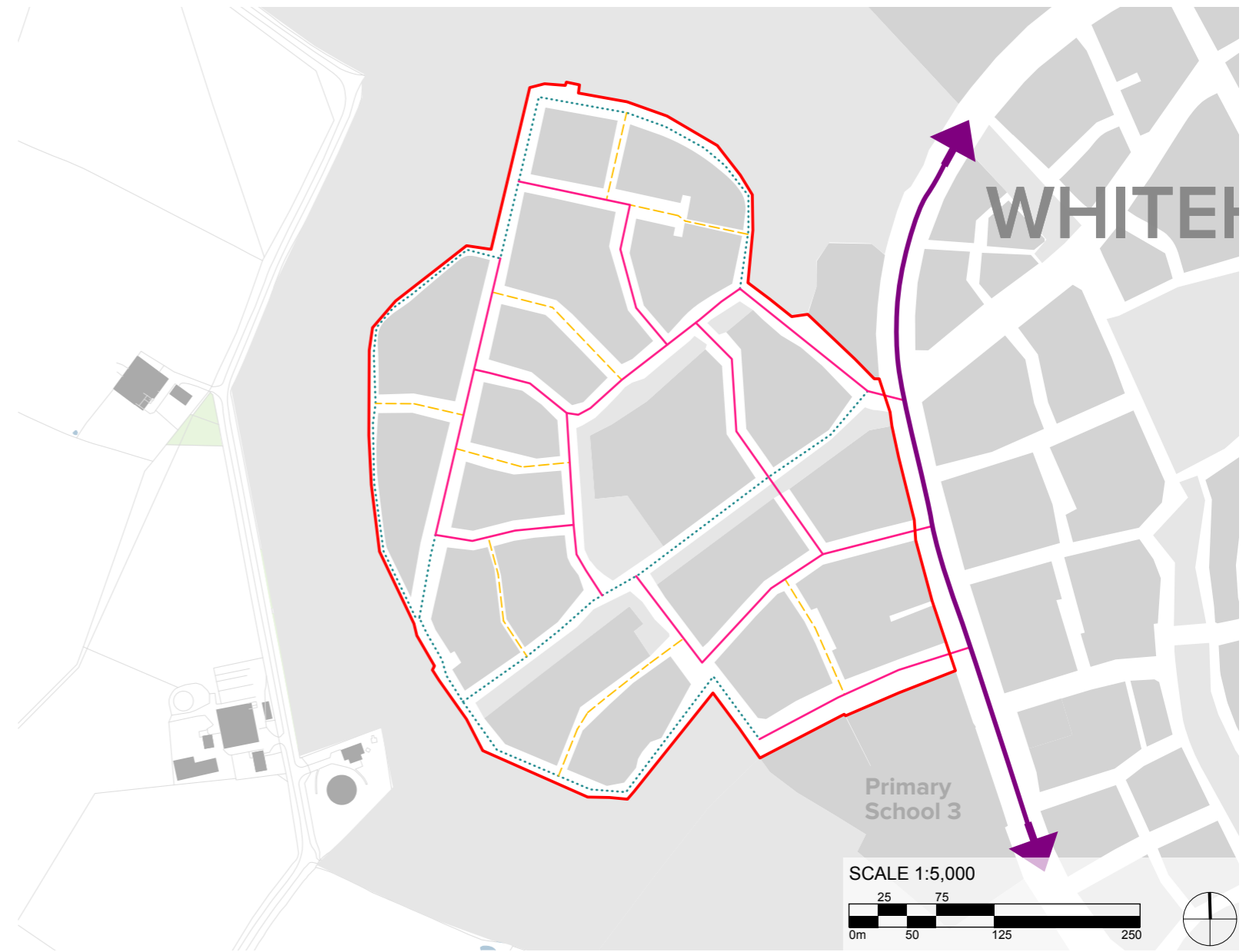
9.1.3. Learning from past experiences, the design process has highlighted a need to code only for the most essential elements of the masterplan and allow flexibility for future Reserved Matters Application layouts. The movement hierarchy, whilst stipulating preferred alignments for primary and secondary routes, identifies a number of options for tertiary routes (as shown below) with the choice over application of tertiary street typology to be determined at Reserved Matters stage.

9.1.4. The City Street and Link Streets (as shown below) form part of the primary infrastructure within Whitehouse South. The delivery of other streets however, which include the Residential Streets, Community Streets, Edge Streets and Mews Streets, will be provided by individual parcels and their precise alignment will be determined at Reserved Matters stage.

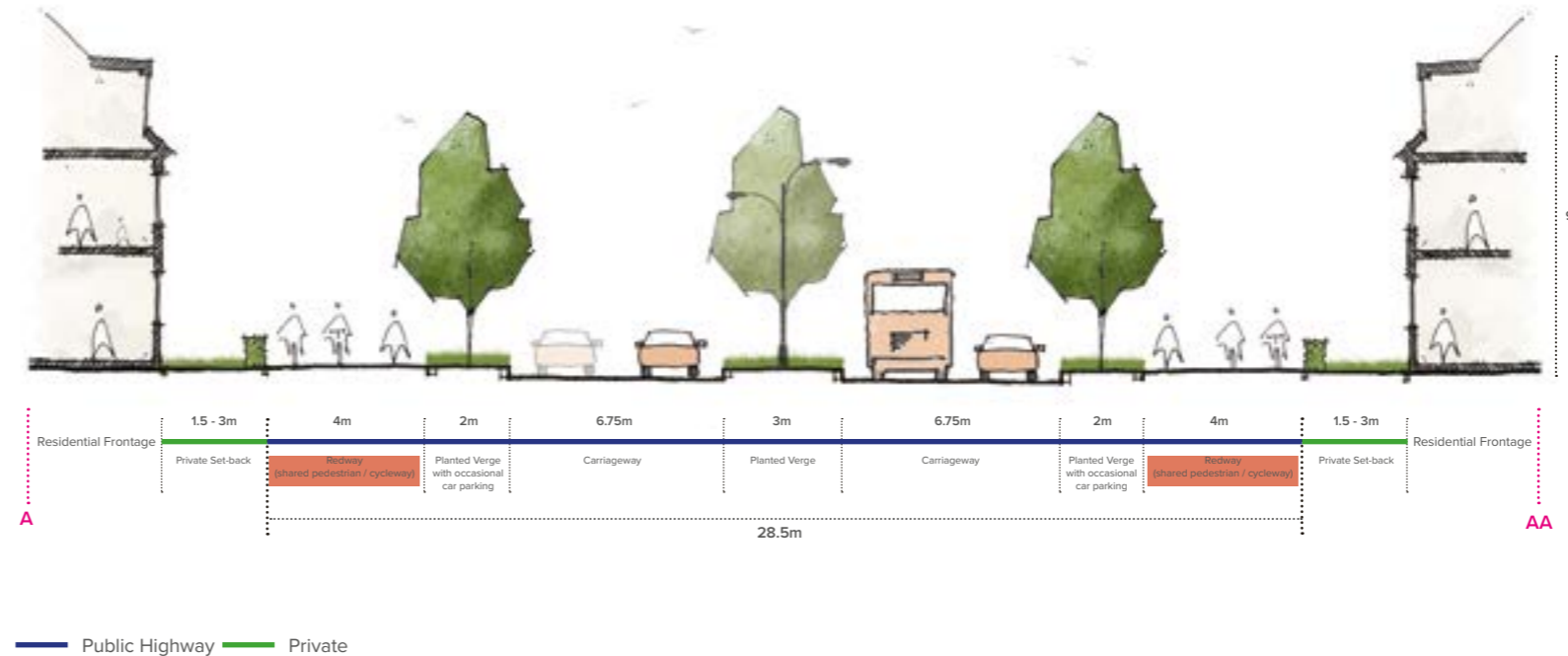
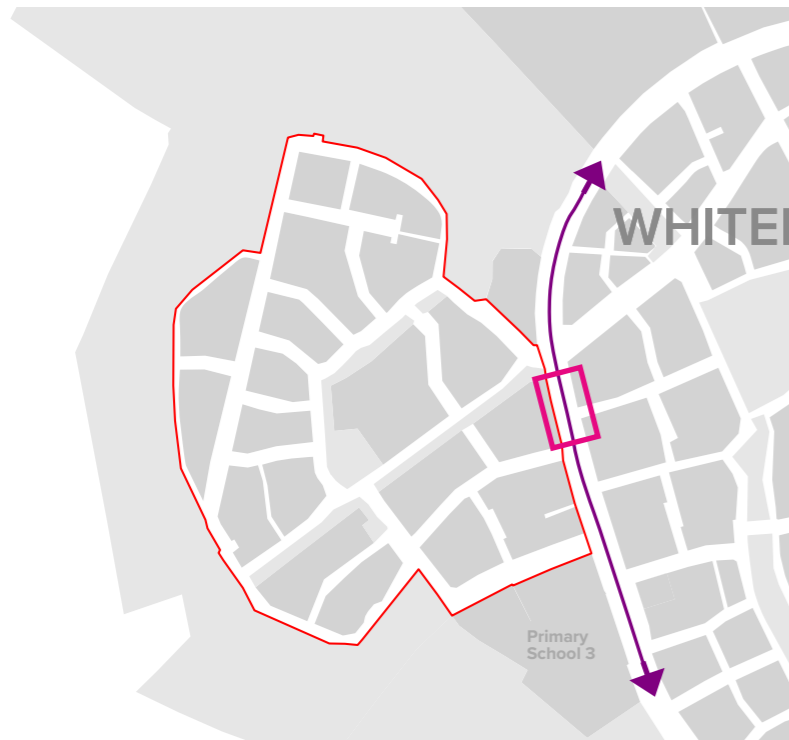
9.1.5. The dimensions and details on street typologies as shown below therefore are in accordance with those shown in the approved Movement Code. Street typologies are shown in this Design Code only to address how built form interacts with the street formation, in terms of building form, building height, frontage treatment, architectural style and treatment and building materials.

FIGURE 6: MOVEMENT NETWORK

- Site Boundary
- Movement Hierarchy**
- City Street
- Residential Street
- Edge Street
- Community Street

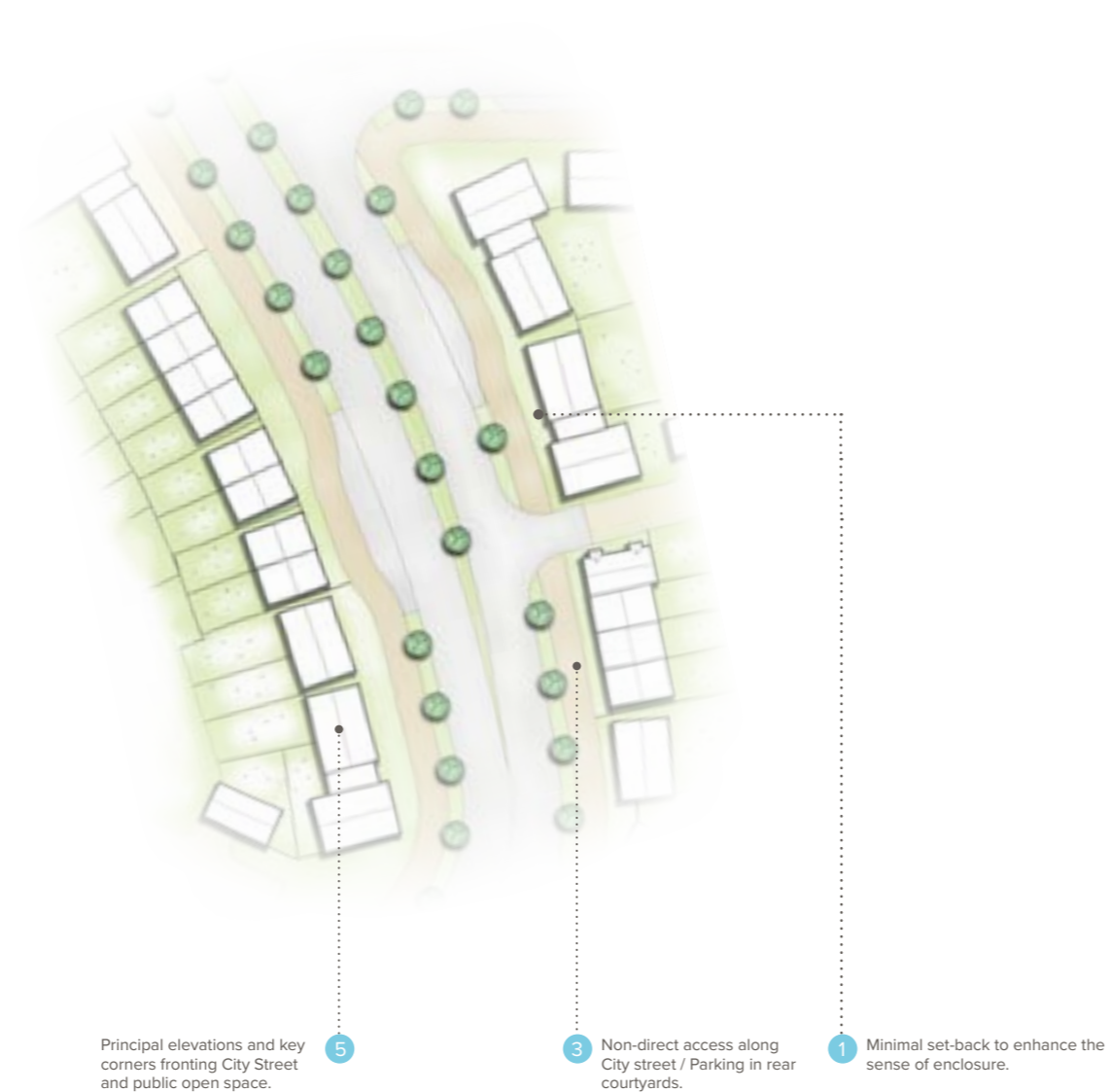


City Street (as approved by the MKWEA Movement Design Code 2008)



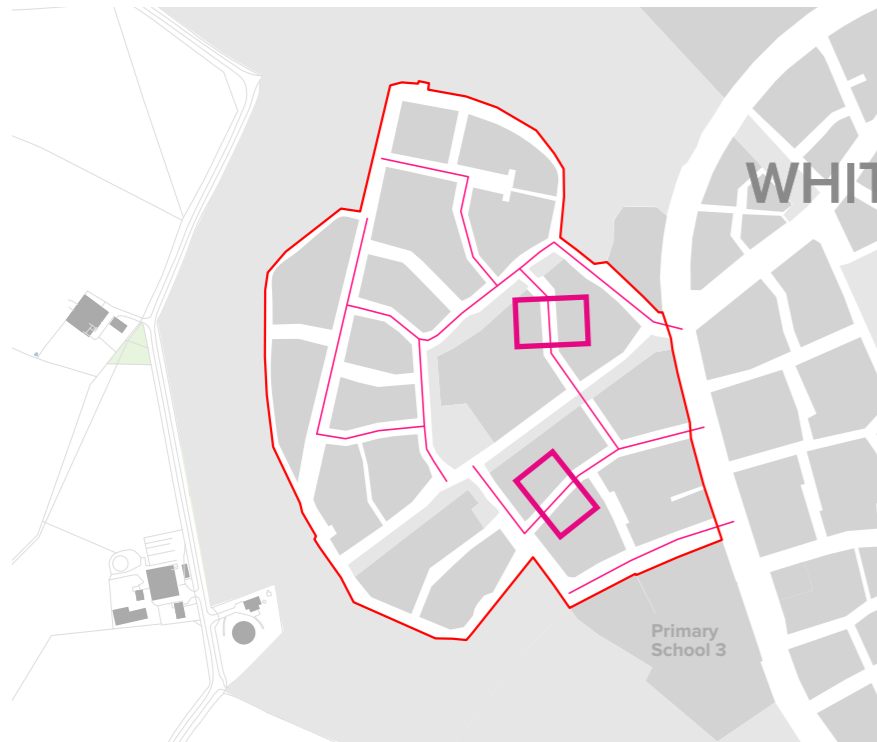
- Providing principal access and the bus route through the development. A boulevard-feel with street trees in central and side verges.
- Dedicated Redways on each side of the corridor included as part of the City Street typology.
- An expectation for substantial similarity of appearance (style, treatment and materials) of buildings on both sides of the City Street, to ensure it feels like “a place” rather than a “boundary between two places”.
- Sufficient on-street parking must be provided and appropriately-located gated pedestrian accesses between dwellings from the city street to parking courtyards to be provided as necessary.

MANDATORY PRINCIPLES



- 1 Building Form**
 - Architecture and hard and soft landscaping in this area needs to be of a modern, high standard.
 - Use of linked-form (e.g. carports/build-overs) and minimal breaks between buildings to contain the street corridor.
 - Principal elevations to face the City Street with minimal set-back to enhance the sense of enclosure.
 - Consistent eaves and roof line to provide consistency.
- 2 Height**
 - 3-4 storeys in general.
 - Taller building forms on corners to emphasise permeability.
- 3 Treatment of Public Highways (in accordance with approved Movement Code)**
 - 2x 6.75m wide carriageway with 3m central verge with tree planting
 - 2m wide verges with trees and occasional parking bays either side of carriageway.
 - 4m redway either side of carriageway.
 - Non-direct access.
- 4 Frontage Types**
 - Setback of 1-3m in accordance with approved Movement Code.
 - Strong definition to built edge provided by a near continuous building line.
 - Use of low hedges/walls to delineate transition between street corridor and semi-private frontage space.
 - All front doors and windows to face onto the street to ensure a strong level of natural surveillance.
 - Sufficient on-street parking must be provided and appropriately-located gated pedestrian accesses between dwellings from the city street to parking courtyards to be provided as necessary.
- 5 Architectural Style & Treatment**
 - Contemporary/Modern styling.
 - Use of projections, recesses and bold colours to animate elevations.
 - Minimalistic approach to window frames deeper window reveals, apartments should have recessed balconies. Projecting metallic frames around windows, no fussy detailing, clean lines, no white window frames.
- 6 Building Materials**
 - Material choices that establish a formal urban character (i.e. more regular and manufactured materials/e.g. wire cut/smooth brick and flat profile tiles).
 - Red or buff-brown brick and dark slate tiles to be used commonly, with limited use of other brick colours and timber cladding as accents.
 - Extra consideration given to key buildings (see below) - for example, bold contemporary statement colours.

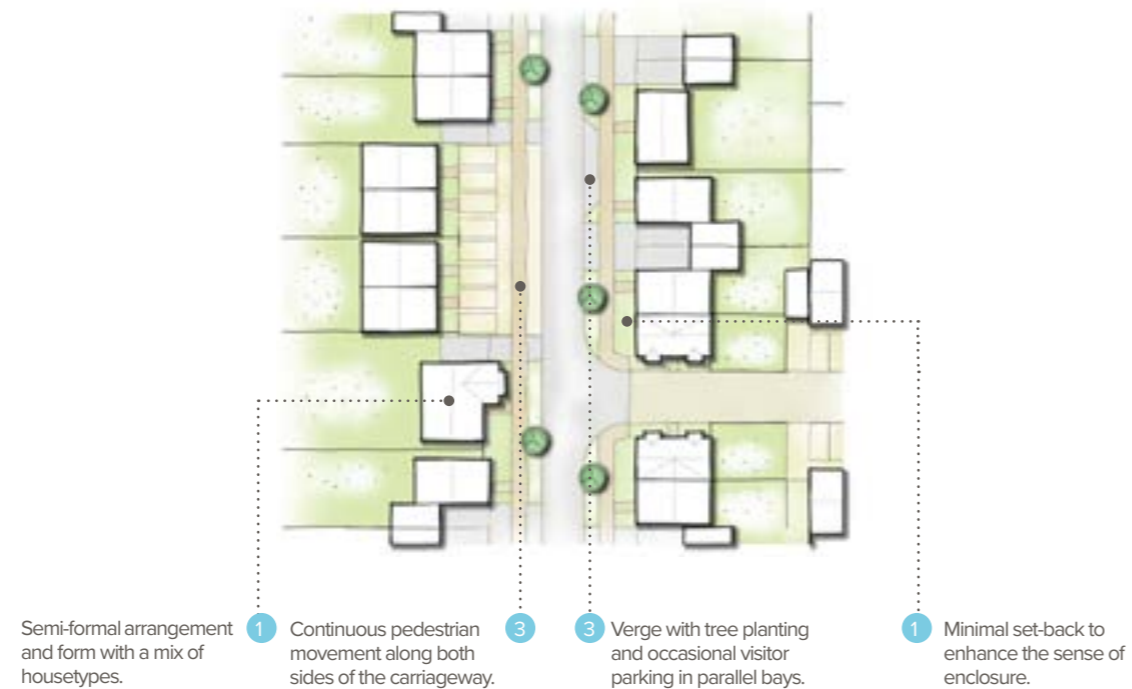
Residential Street (as approved by the MKWEA Movement Design Code 2008)



- Route that connects the City Street and Link Street to smaller tertiary routes.
- Street trees provided along the whole route in verges.
- Visitor parking bays provided between street trees where appropriate.
- On-plot parking spaces should be placed behind the building line where possible.



MANDATORY PRINCIPLES



1 Building Form

- Semi-formal arrangement and form using a mix of housetypes to create a transition in character from the formal City Street to less formal areas.
- Principal elevations facing the street with minimal set-back to enhance the sense of enclosure.
- Consistent eaves and roof line to provide consistency.

2 Height

- 2-2.5 storeys in general.
- Taller building forms on corners to emphasise permeability.

3 Treatment of Public Highways (in accordance with approved Movement Code)

- 5.5m wide carriageway
- 2m wide planted verges with trees and occasional parking bays either side of carriageway.
- 2m wide footway either side of carriageway.

4 Frontage Types

- Setback of 1.5-3m in accordance with approved Movement Code.
- Use of low hedges/walls to delineate transition between street corridor and semi-private frontage space.
- All front doors and windows to face onto the street to ensure a strong level of natural surveillance.
- Front boundary treatments must be consistent along streets in terms of height, set back and appearance.

5 Architectural Style & Treatment

- Contemporary/Modern styling.
- A variety of modern building styles, colours and materials to create a successful residential composition.
- Use of projections, recesses and bold colours to animate elevations.

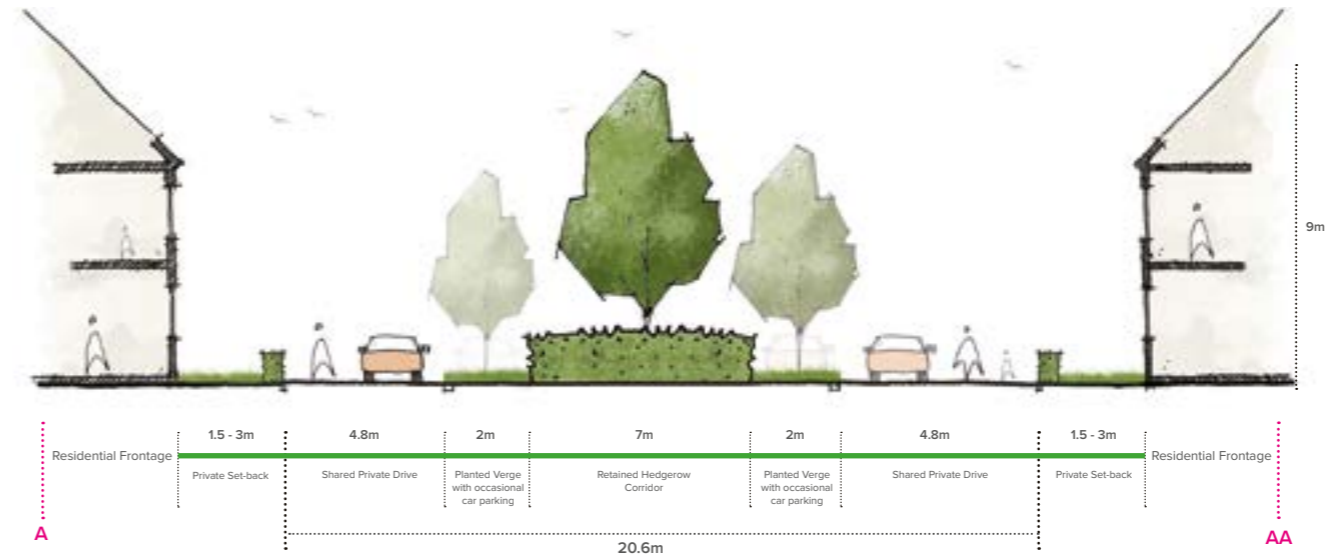
6 Building Materials

- Material choices that establish a transition in character between formal streets (i.e. City Street and Link Street) and less formal tertiary streets (i.e. less regular/rough cut brick and roof tile types).
- Red or buff-brown brick and dark slate tiles to be used commonly, with limited use of other brick colours and timber cladding as accents.
- Extra consideration given to key buildings (see below) - for example, bold contemporary statement colours.

Edge/Green Street (as approved by the MKWEA Movement Design Code 2008)



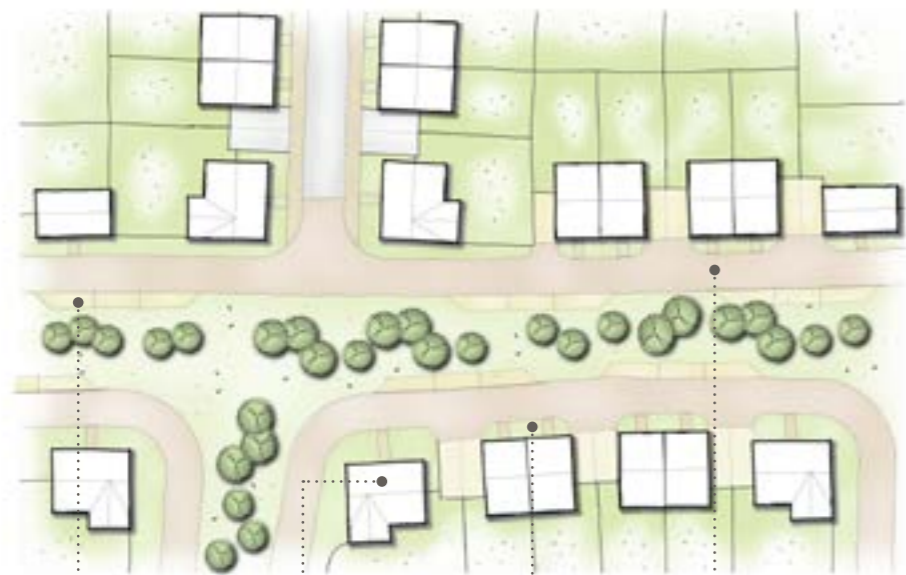
- Tertiary street provided fronting onto Green Corridors and the edges of the development. Street trees provided along the whole route in verges.
- Visitor parking bays provided between street trees where appropriate.
- On-plot parking spaces should be placed behind the building line where possible.



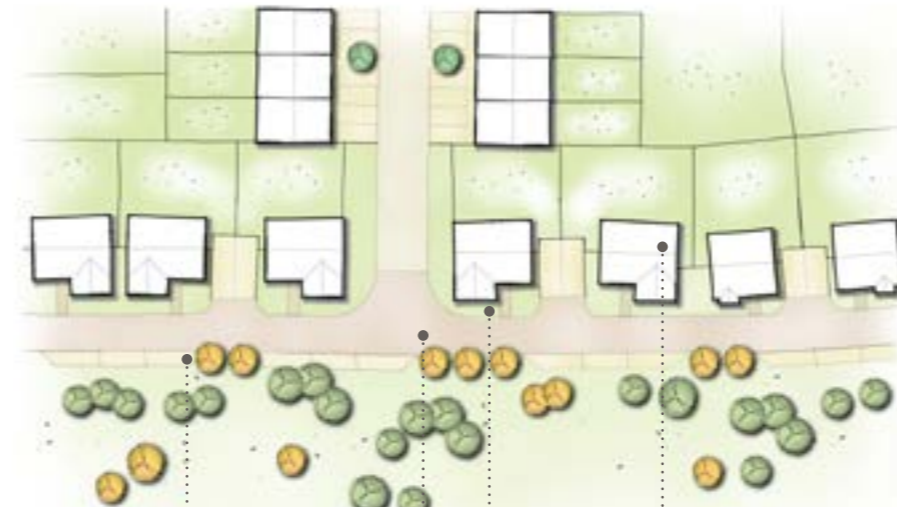
OPTION 1 (GREEN CORRIDOR)



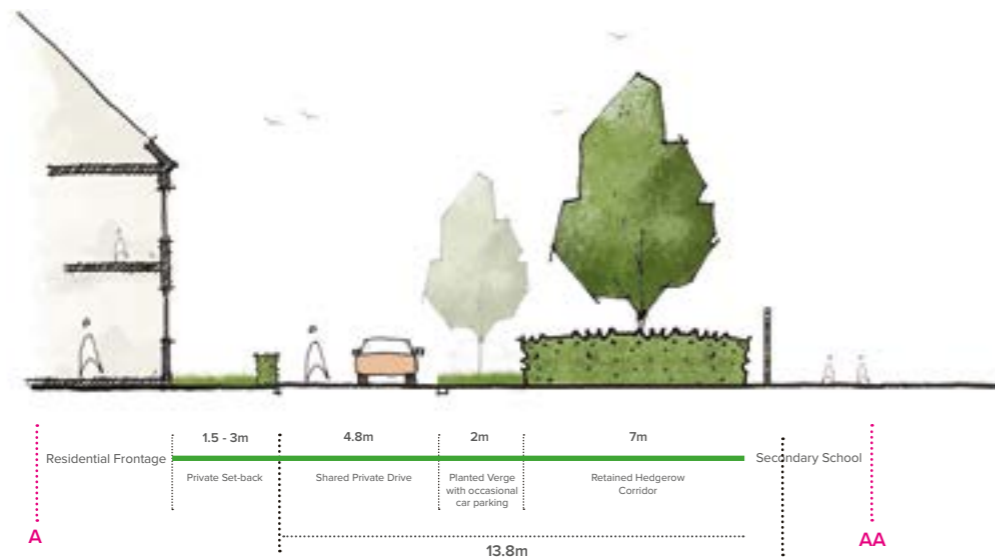
OPTION 2 (PARKLAND EDGE)



- 3 Visitor parking in parallel bays on the hedgerow side of the street.
- 1 Detached and semi-detached built form to create an informal response to the edge.
- 1 More generous set-back.
- 3 Single material shared surface (carriageway, pavement and parking bays).



- 3 Visitor parking in parallel bays on the edge side of the street.
- 3 Single material shared surface (carriageway, pavement and parking bays).
- 1 More generous set-back.
- 1 Detached and semi-detached built form to create an informal response to the edge.

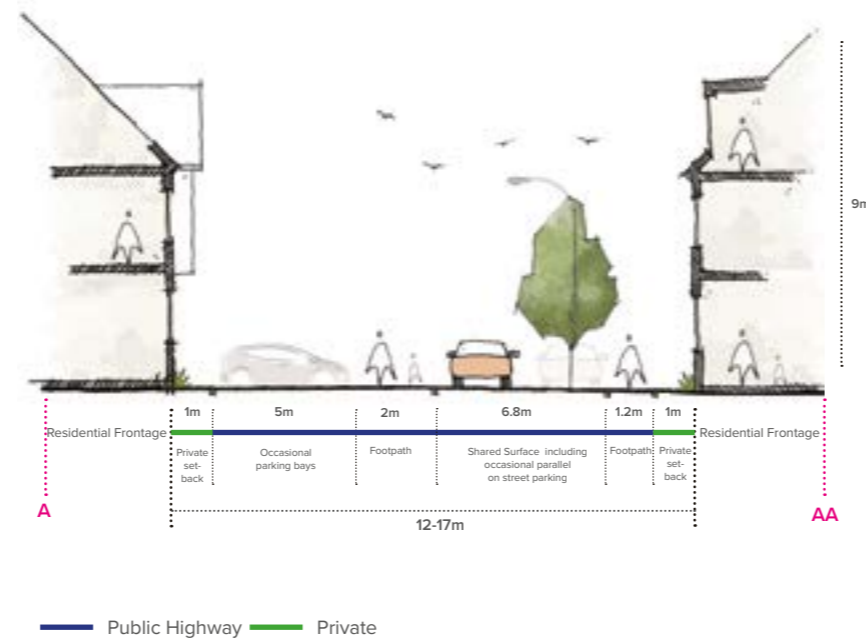
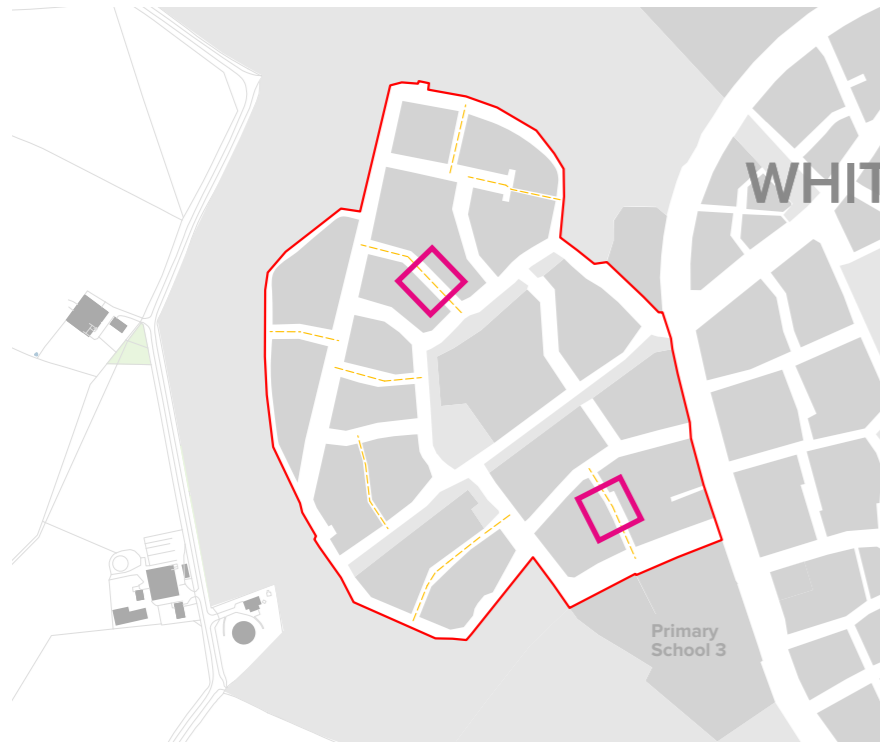


MANDATORY PRINCIPLES

- 1 Building Form**
 - Detached and semi-detached built form with deliberate gaps to create a looser informal response to the edge.
 - Principal elevations facing the edge street with generous set-back to ensure a transition between built form and the green edge.
 - Consistent eaves and roof line to provide consistency.
 - Planting between units to provide an informal response to the adjacent natural character of mature hedgerows.
 - Careful consideration will need to be given to the appearance/detailing of side elevations where these are exposed.
- 2 Height**
 - 2-2.5 storeys.
 - Taller building forms on corners to emphasise permeability.
- 3 Treatment of Public Highways (in accordance with approved Movement Code)**

CONDITION 1 (hedgerow): <ul style="list-style-type: none"> - 4.8m wide shared private drives fronting onto green corridor. - 7m wide retained hedgerow corridor with 2m wide planted verges either side. - 2m occasional parking bays on hedgerow side. 	CONDITION 2: <ul style="list-style-type: none"> - 4.8m wide shared private drive fronting onto edge. - 2m occasional parking bays on edge side.
---	--
- 4 Frontage Types**
 - Setback of 1.5-3m in accordance with approved Movement Code.
 - Use of low hedges/walls to delineate transition between street corridor and semi-private frontage space.
 - All front doors and windows to face onto the street to ensure a strong level of natural surveillance.
 - Front boundary treatments must be consistent along streets in terms of height, set back and appearance.
- 5 Architectural Style & Treatment**
 - Contemporary/Modern styling that draw upon traditional forms.
 - Use of projections, recesses and brick detailing (e.g. hit and miss, alternate coloured courses) to add interest to elevations.
- 6 Building Materials**
 - Distinctive use of materials that reflects the natural character of the setting (i.e. less regular/rough cut brick and roof tile types).
 - Red brick and timber cladding with limited limestone, and clay tiles as an accent material.

Community Street (as approved by the MKWEA Movement Design Code 2008)



Perpendicular parking bays in front of the properties. 3

Single material shared surface. 3

1 Formal arrangement and minimal set-back to enhance the sense of enclosure.

3 Occasional parallel on street parking.

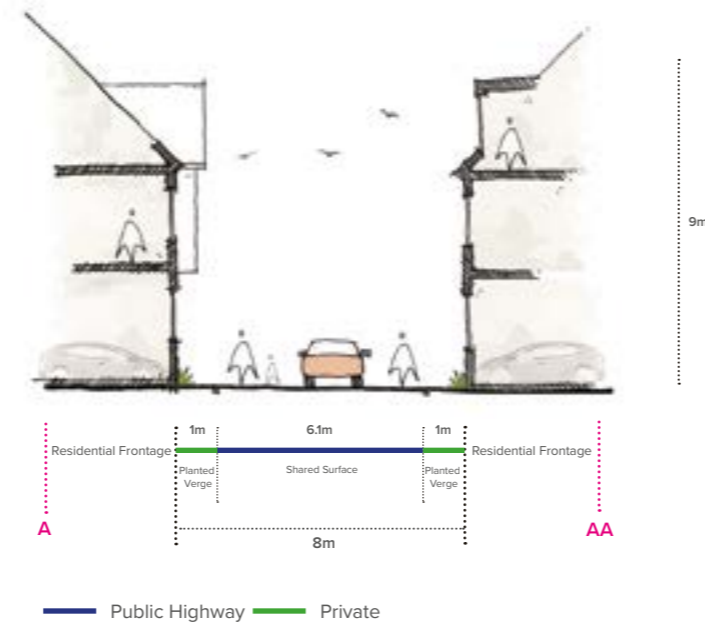
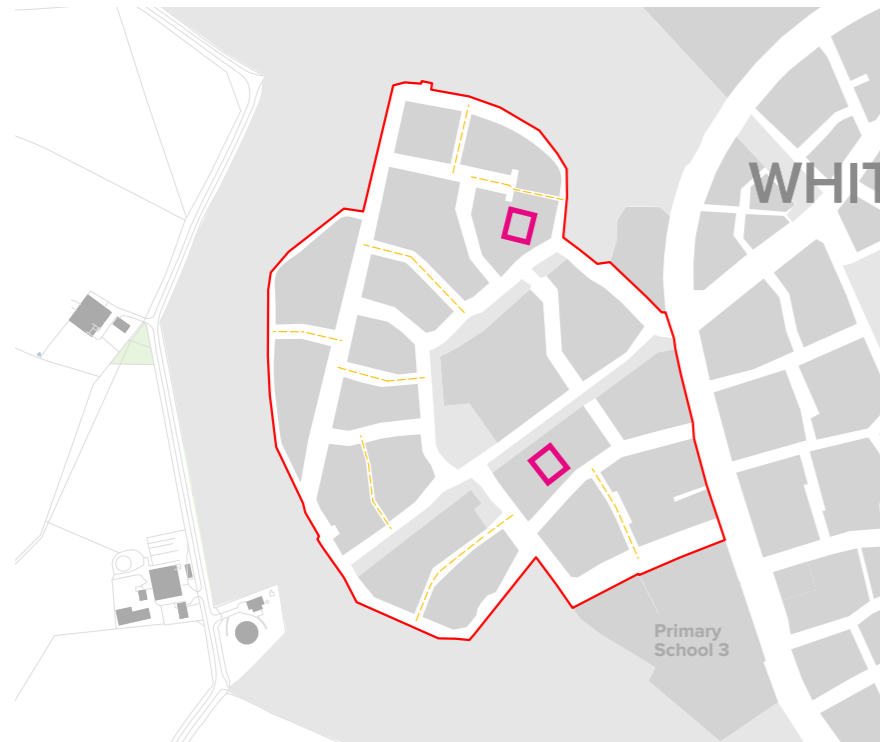
- Common tertiary/minor street typology.
- Provides a safe residential environment with a common surface treatment across the corridor that engenders slower vehicle speeds.
- Minimal carriageway width with occasional visitor parking in order that parked do not dominate the street scene.
- On-plot parking spaces should be placed behind the building line where possible.



MANDATORY PRINCIPLES

- 1 **Building Form**
 - Formal arrangement and form using a mix of housetypes arranged in intimate groupings.
 - Principal elevations facing the street with minimal set-back to enhance the sense of enclosure.
 - Consistent eaves and roof line to provide consistency.
- 2 **Height**
 - 2-2.5 storeys in general.
 - Taller building forms on corners to emphasise permeability.
- 3 **Treatment of Public Highways (in accordance with approved Movement Code)**
 - 6.8m wide shared surface including occasional parallel on street parking.
 - 2m wide and 1.2m wide footpaths either side of shared space.
 - 5m length perpendicular parking bays in groupings of no more than 3 separated by planted strips.
- 4 **Frontage Types**
 - Setback of max. 1m incorporating planted strip using climbing species to add greenery to elevations and street scene.
 - All front doors and windows to face onto the street to ensure a strong level of natural surveillance.
 - Front boundary treatments must be consistent along streets in terms of height, set back and appearance.
- 5 **Architectural Style & Treatment**
 - Use of projections, recesses and brick detailing (e.g. hit and miss, alternate coloured courses) to add interest to elevations.
 - Varied roof form and arrangement to create an interesting roofscape within blocks.
- 6 **Building Materials**
 - Red or buff-brown brick and dark slate tiles to be used commonly, with limited use of other brick colours and timber cladding as accents.
 - Extra consideration given to key buildings (see below) - for example, bold contemporary statement colours.

Mews Street (as approved by the MKWEA Movement Design Code 2008)



- Intimate setting with built form hard up to the edge of the street typology.
- Provides a safe residential environment with a common surface treatment across the corridor that engenders slower vehicle speeds.
- On-plot parking spaces should be placed behind the building line where possible.



MANDATORY PRINCIPLES

- 1 **Building Form**
 - Formal arrangement and form using a mix of housetypes arranged in intimate groupings.
 - Principal elevations facing the street with minimal set-back to enhance the sense of enclosure.
 - Consistent eaves and roof line to provide consistency.
- 2 **Height**
 - 2 storeys.
 - Taller building forms on corners to emphasise permeability.
- 3 **Treatment of Public Highways (in accordance with approved Movement Code)**
 - 6.1m wide shared surface including occasional parallel on street parking.
- 4 **Frontage Types**
 - Setback of max. 1m incorporating planted strip using climbing species to add greenery to elevations and street scene.
 - All front doors and windows to face onto the street to ensure a strong level of natural surveillance.
 - Front boundary treatments must be consistent along streets in terms of height, set back and appearance.
- 5 **Architectural Style & Treatment**
 - Use of projections, recesses and brick detailing (e.g. hit and miss, alternate coloured courses) to add interest to elevations.
 - Varied roof form and arrangement to create an interesting roofscape within blocks.
- 6 **Building Materials**
 - Varied material and colour choices to add visual interest.
 - No over-dominance of one particular brick type or colour.
 - Extra consideration given to key buildings (see below) - For example, bold statement colours.

10. Parking Typologies (as approved by the MKWEA Movement Design Code 2008)

10.1. Overview

10.1.1. The parking typologies in this Design Code follow the precise specifications and details for each parking typology as already set out by the approved MKWEA Area 10 Movement Design Code (September 2008).

10.1.2. The Design Code is not prescriptive about which parking typologies will appear where in the Design Code area. It is however noted that different housing typologies lend themselves toward different parking solutions. The parking typologies identified are mandatory where applicable to a specific housetype proposed. The aim of providing parking in convenient locations should not be detrimental to the landscaping within the street scene.

10.1.3. Detached and semi-detached houses allow for on-plot parking to the side of the dwelling. Parking spaces should however be such that parking spaces do not protrude beyond the building frontage line to ensure that cars do not dominate the street scene.

10.1.4. Wider-fronted terraced units can accommodate integrated parking so long as this does not result in sterile ground floor elevation. Narrower terraced units can be set back from the building frontage to accommodate parking in front of them.

10.1.5. Rear or side court parking must only be used when all other on-street and on-plot options have been exhausted. Where parking courts are required, they should be limited in size and incorporate trees and planting to soften the amount of hard-surfacing. As a general rule, a maximum of 4 parking spaces should be separated by planted strips.

10.1.6. The examples shown right provides examples of the type of parking solutions that are considered appropriate for the variety of street types across Whitehouse South. With reference to on-street parking options, the table needs to be read in conjunction with the approved Movement Design Code. In particular regard to the City Street, sufficient on-street parking must be provided and appropriately-located gated pedestrian accesses between dwellings from the city street to parking courtyards to be provided as necessary.



Parking to the side

Car parking provided to the side is the preferred parking arrangement for most street typologies as it allows buildings to relate positively and directly with the street without being interrupted by cars. Car parking spaces must be set behind the building line in order for the approach to work effectively. Where car parking is provided to the side of properties, spaces should be observable from a ground floor window to ensure adequate surveillance.



Parking to the front

For tighter forms of housing (e.g. terraced housing), car parking spaces provided to the front of the property will be suitable. Surface treatments should coordinate as part of the wider palette of materials for adjacent footways etc. and sufficient space should be provided between building frontages and parking spaces that allow for a landscaped strip (min. 0.5m depth).



Integral Parking

Car Ports will be used to create linked built form either where needed for acoustic mitigation or to contain spaces. Essentially this solution provides a variant of 'parking to the side' with the inclusion of accommodation over side spaces to create continuous built form. It is essential that at least one-storey of accommodation is provided over car ports to create linked built form as required. Car Ports should be overlooked by a window in a side elevation.



Parking Courts

Parking courts are not usually preferable but will be considered acceptable where there are restrictions to providing vehicular access off the street (e.g. on the City Street and any frontage with a redway in front of it).

Apartments served by parking courts should have a convenient, prominent main entrance onto the parking area (e.g. dual entrances front and rear from a stairwell or lobby area). Houses should not be served by parking courts other than in the most exceptional circumstances (e.g. where access to the "front" street is as convenient as access to the "rear" parking area). For parking courts serving houses, electric lockable gates will be required, boundary treatments should allow for surveillance and courtyards should be well-lit.

No visitor spaces should be provided within parking courts and any dwelling served by a parking courtyard will be required to increase to 0.5 spaces visitor spaces per dwelling. One-sided 'rear parking' streets should be avoided where possible. In some instances apartments above garages could be used to hide parking areas and provide two sides to the street.

Picture above shows a parking court already constructed and in use at MKWEA Area 10.

11. Key Spaces

11.1. Overview

11.1.1. Within MKWEA, various key spaces exist that provide a focus for activity and fulfil a variety of functions and requirements within the wider masterplan area.

11.1.2. Whitehouse South Green is located within the centre of Whitehouse South to serve the new community within this part of the development. There is a need for the Mandatory Principles for Whitehouse South Green to interface with Mandatory Principles for Street Typologies. In cases such as this, Reserved Matters designers should strike a balance between the requirements of the respective Mandatory Principles of streets and spaces to reach the best outcome. For example, ensuring a coordinated and considered approach to surface materials.

FIGURE 7: KEY SPACES





Whitehouse South Neighbourhood Green





MANDATORY PRINCIPLES

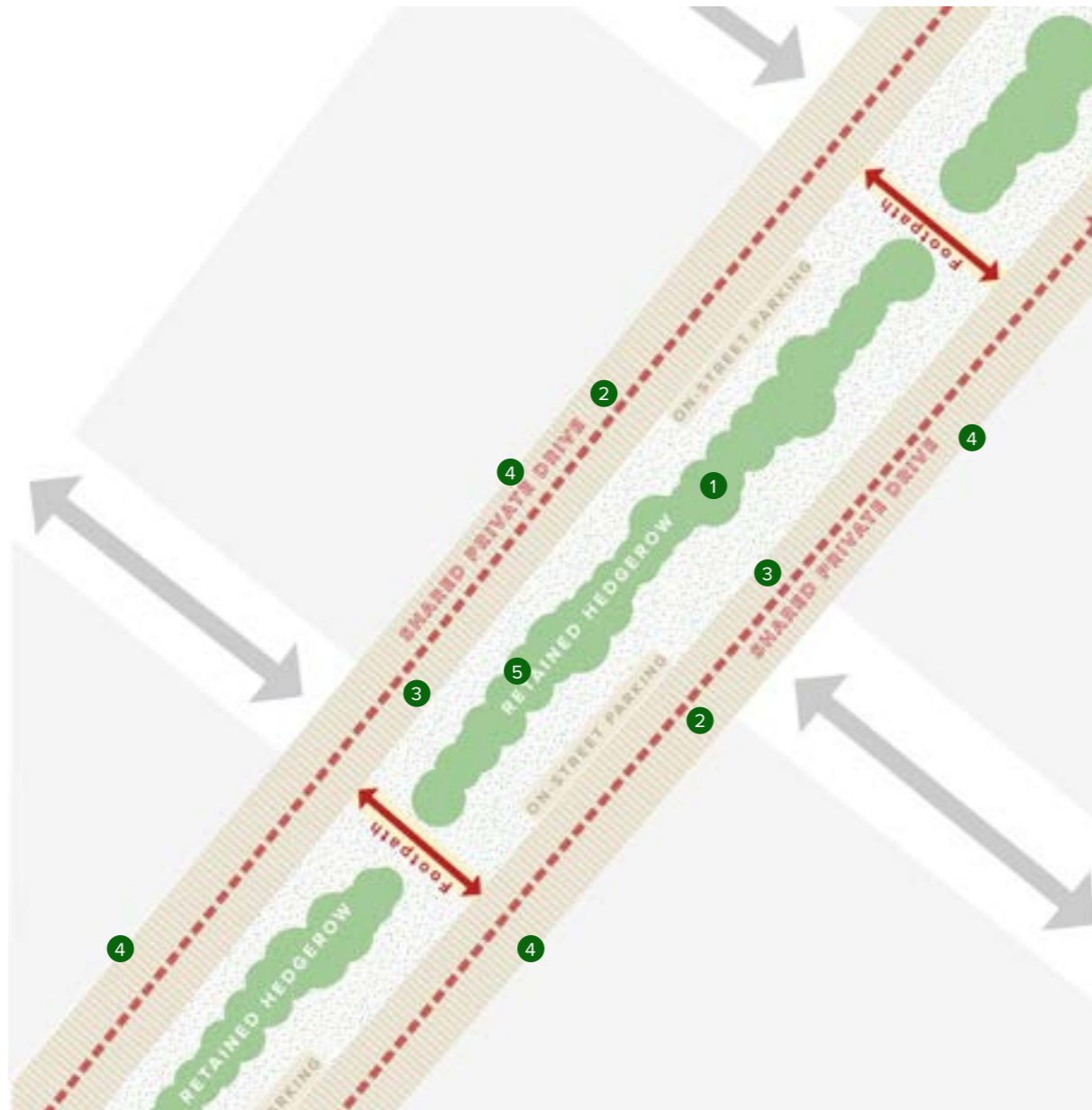
For built form responses to the Whitehouse South Neighbourhood Green see **'Residential Street'** and **'Edge Street'** typologies above.

- 1 Formal gardenesque space with mown areas of amenity grass bordered by planted beds and ornamental trees and gravel footpaths.
- 2 Central seating area shaded by formally-arranged tree grouping.
- 3 Well defined linear paths to central area to reinforce the formality of the space.
- 4 Boundary hedges to provide enclosure to the space.
- 5 Active frontage with houses facing directly onto the space.
- 6 Approach to lighting should provide a low key and uncluttered solution and should take account of key technical considerations such as ecology and crime prevention. Detailed lighting design will be subject to condition.



Green Corridors





MANDATORY PRINCIPLES

For built form responses to the Whitehouse South Neighbourhood Green see ['Residential Street'](#) and ['Edge Street'](#) typologies above.

- 1 Retained hedgerows preserved to retain ecological and landscape interests set within a generous grassed verge to create a green, natural and attractive edge to development.
- 2 Well-defined linear route with buildings following a regular line to frame the visual axis.
- 3 Pedestrian/cycle routes along edge streets to Green Corridors with direct links to local centre and redways to assist in sustainable movement through the development.
- 4 Active frontage with houses facing footpaths and narrow shared streets.
- 5 Approach to lighting should provide a low key and uncluttered solution and should take account of key technical considerations such as ecology and crime prevention. Detailed lighting design will be subject to condition.

12. Key Landmarks & Frontages

12.1.1. Landmark buildings should be notably distinct within the wider scheme and use additional detailing and accent materials to emphasise particular house-types within their setting.

1.1.1. Such buildings are characterised by their location in relation to the site and are typically highly visible and hold a commanding position that stand out from the context and the neighbourhood, bringing focus and identity. The most appropriate locations for Landmarks are identified on the Placemaking Plan. These locations have been selected in line with the following principles:

- They are in highly visible locations within the pattern of streets and spaces.
- They would be appropriate landmarks for navigation.
- They hold a commanding position that is not shared by other buildings.
- They are distributed throughout the plan in such a way that important pedestrian and vehicular nodes and routes become more memorable.

1.1.2. In order to ensure Landmarks become exemplars, innovative, bold and imaginative design responses are required that are appropriate to their settings. In order to achieve this, architectural considerations may include:

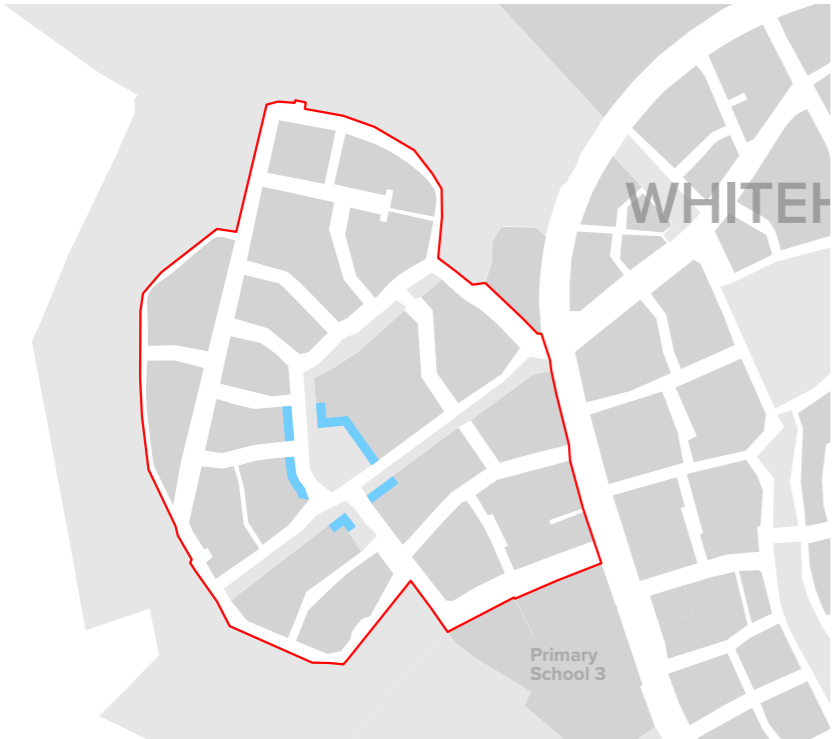
- Reinforcing the character of a particular area in which the building is found.
- Full height windows to provide a vertical proportion.
- Gables and roof details that imply a vertical emphasis.
- Details that emphasise the corner position of the building.
- Increased proportions of facade glazing.
- Bespoke balconies, porches and screens in contrasting materials.

FIGURE 8: KEY LANDMARKS & FRONTAGES

- Site Boundary
- Landmarks & Frontages
 - Key Corners
 - Vista Termination
 - Whitehouse South Green Key Frontage

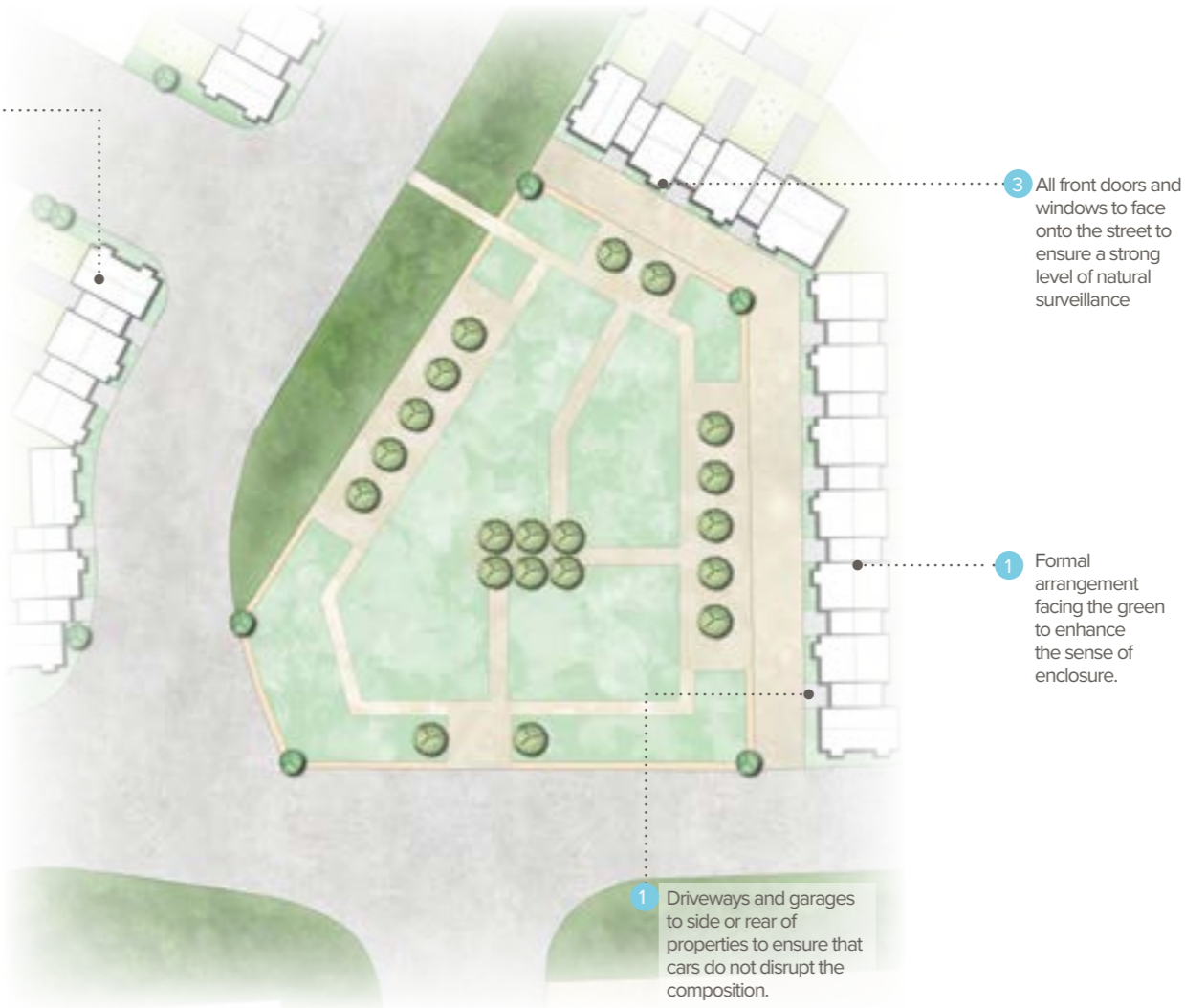


Whitehouse South Green Key Frontage



Dwellings will be focused on facing Whitehouse South Green in order to ensure the success of the space as a key part of the public realm within the development.

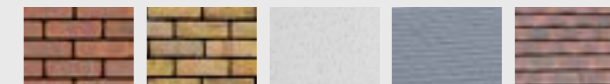
A limited palette of materials with subtle variation in form and orientation of building features (e.g. facing gables) will provide a successful composition as a backdrop to the public realm.



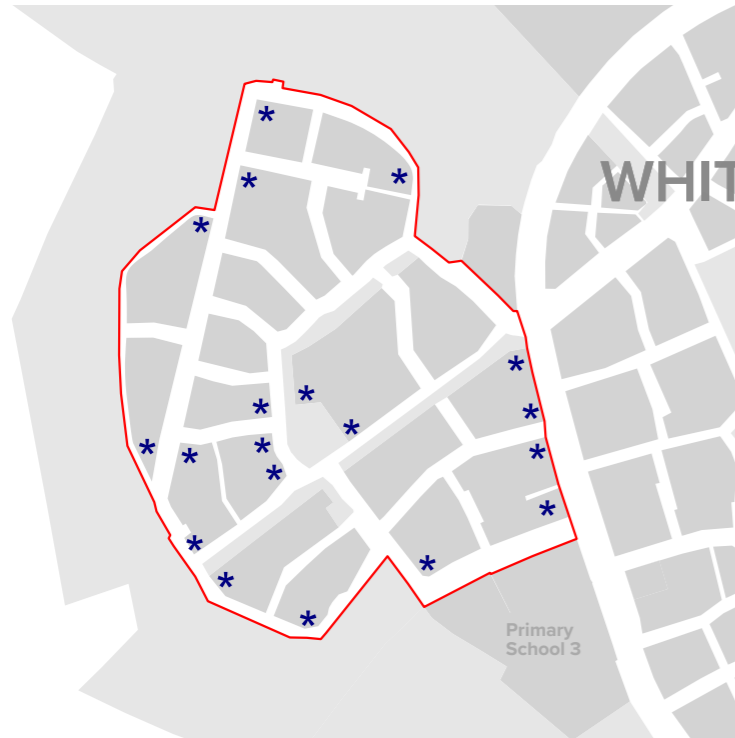


MANDATORY PRINCIPLES

- 1 Form & Layout**
 - Consistent building frontage facing Whitehouse South Green or the street to ensure a positive relationship and emphasise enclosure of the public realm. Blank façades (e.g. garages) will not be acceptable on any of the key frontage.
 - Driveways and garages to side or rear of properties to ensure that cars do not disrupt the composition. Parking to be positioned no further than the building frontage line to ensure dwelling elevation is the most dominant.
 - Subtle variation of roof orientation with occasional facing gables onto the public realm to break the uniformity.
- 2 Height**
 - 2-2.5 storeys with variation through the composition.
- 3 Frontage Types**
 - Setback of max. 3m with front gardens and boundary treatments that complement the landscape design of the adjacent Whitehouse South Green.
 - All front doors and windows to face onto the street to ensure a strong level of natural surveillance.
- 4 Architectural Style & Treatment**
 - Contemporary/Modern styling that draw upon traditional forms.
 - Notable upstep in architectural treatment from surrounding character area to emphasise hierarchical importance of the frontage.
 - Use of projecting porches and windows, contemporary box dormers, recesses and brick detailing (e.g. hit and miss, alternate coloured courses) to add interest to elevations.
 - Obvious rhythm created through the street scene through use of repeated architectural details (e.g. porches, projecting windows)
- 5 Building Materials**
 - Variation within a limited palette of red/warm and buff brick and light render, slate and/or clay roof tiles with a well-considered elevational treatment through all built form in the frontage to create composition within the space.



Key Corners



- Corner landmarks are typically more noticeable and have a heightened responsibility in respect of legibility and allowing a permeable network to be successful.
- Corners should always positively face each street that they address by providing active frontages (front doors and windows) onto the public realm.
- Minor Corners should apply the same principle in a more subtle way reducing their mass at the corner to ensure the building enclosure is not overbearing.





MANDATORY PRINCIPLES

1 Form & Layout

- All buildings located on identifiable corners (where two routes, two spaces, or a route and a space meet) must positively address both directions through positioning of entrances, generous windows to habitable rooms, glazed bays/projections and upper level balconies where appropriate.
- Blank façades will not be acceptable on corners.
- Fenestration to both primary and secondary frontage must be to habitable rooms to provide surveillance (bay windows being particularly successful in this regard), and be co-ordinated and ordered as one composition.
- Driveways and garages to side or rear of properties to ensure that cars do not disrupt the corner arrangement. Parking to be positioned no further than the building frontage line to ensure dwelling elevation is the most dominant.
- Building corners to more intimate streets to reduce in mass at the corner to ensure building enclosure is not overbearing.

2 Height

- 2-2.5 storeys with the tallest or largest element of the building mass being located directly on that corner.

3 Frontage Types

- Setback of max. 1m incorporating planted strip using climbing species to add greenery to elevations and street scene.
- All front doors and windows to face onto the street to ensure a strong level of natural surveillance.

4 Architectural Style & Treatment

- Use of projections, recesses and brick detailing (e.g. hit and miss, alternate coloured courses) to add interest to elevations.
- Where private rear garden boundary is exposed to a public space or streets, this must sit in the alignment of the building line and be build of the same material as the building (i.e. brick wall).
- Where two dwellings are used to turn a corner in a semi-detached perpendicular arrangement (i.e. not 45-degree), contrasting material should be used on the dwelling facing the secondary or tertiary street frontage.

5 Building Materials

- Corner landmarks should include the same materials/colours that can be found on buildings on the streets that they front onto as well as alternate materials that complement these.

Vista Termination



- Vista Termination Landmarks are positioned deliberately at an intended termination of a direct view along a street or path; as such they are more visible than most buildings.
- It is essential that Vista Termination Landmarks are designed in such a way that recognises the likelihood of building being viewed.
- Views should not terminate on non-primary façades, including blank, semi-blank or uncoordinated elevations, or non-habitable structures (garages or car ports).





MANDATORY PRINCIPLES

1 Form & Layout

- Where linear spaces or routes establish a vista, that vista will be terminated by a carefully positioned building.
- Vista termination landmarks to be located in such a position that they would be viewed fully. Vista termination landmarks must not terminate in a view of, for example, a private driveway or garage door, or the side boundary wall to a plot.
- Vista termination built form can be composed of one dwelling or a series of dwellings but must be designed as one and in such a way that they compliment the overall vista.
- Principal building façades should be orientated down the vista.
- Driveways and garages to side of vista termination properties to ensure that cars do not disrupt the vista. Parking to be positioned no further than the building frontage line to ensure dwelling elevation is the most dominant.

2 Height

- 2-2.5 storeys with an increase in scale from surrounding buildings possible either through storey numbers or building proportions where this is appropriate.

3 Frontage Types

- Up to 3m front garden depths.
- All front doors and windows to face down the vista.

4 Architectural Style & Treatment

- Use of gables to emphasise mass on the view termination point.
- Bay windows, at ground, or ground and first floor, potentially with balconies (if appropriate).

5 Building Materials

- Vista termination landmarks should include the same materials/colours that can be found on buildings on the streets that they front onto as well as alternate materials that complement these.

13. Materials Palette

13.1. General Approach

13.1.1. A general approach to materials is provided that will allow further detailed discussion to take place at Reserved Matters Application stage. The overarching mandatory principle is of consistency in materials on primary and secondary routes (i.e. City Streets, Link Streets and Residential Streets) with greater flexibility on tertiary streets (Edge/Green Streets, Community Streets and Mews Streets) unless Mandatory Principles outlined above stipulate otherwise.

13.1.2. The Design Code does not seek to address architectural details (such as doors, canopies, eaves), however these will be required to be co-ordinated into a cohesive solution for each Reserved Matters Application. There must also be a co-ordinated approach between developers within the same character areas.

13.1.3. The Milton Keynes Residential Development Guide SPD (2012) recognises that there is no traditional dominant local building material in Milton Keynes. It advises that a simplistic approach to materials should be taken, both in type of materials used and the extent of the palette.

13.1.4. To ensure a coherent street frontage, the SPD recommends that:

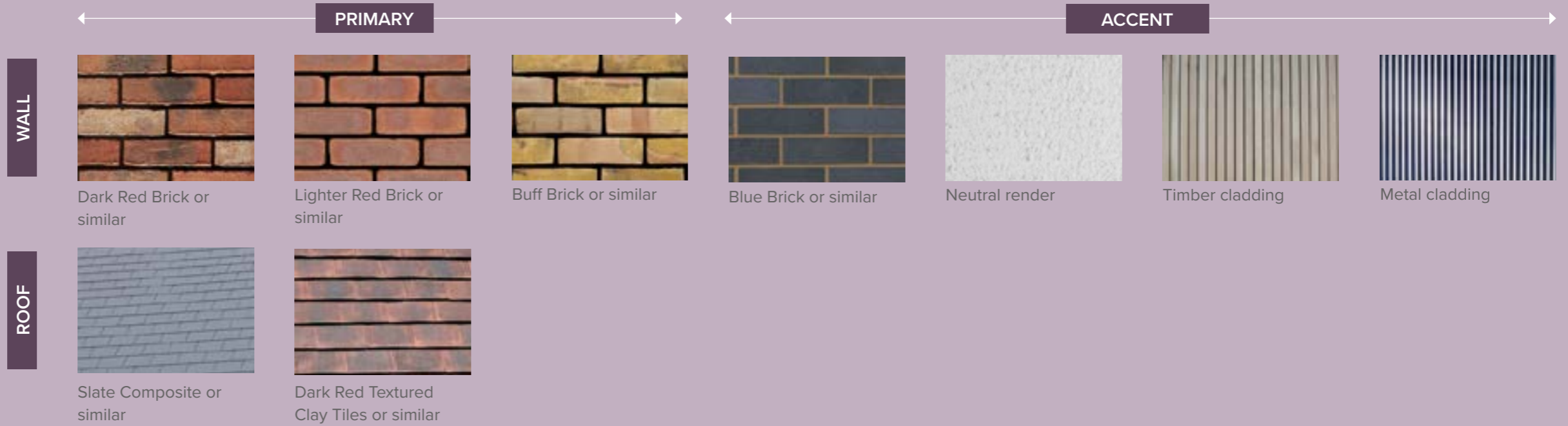
- the number of materials is restricted (to a single primary material and a small number of accent materials);
- the same materials are employed in the same parts of the facade or frontage; and

- no more than 3 facing materials should be used per elevation or street frontage.

13.2. Indicative Palette

13.3. The table right indicates the mandatory palette of external materials that would be considered to be acceptable for new development at Whitehouse South. Primary and accent materials are suggested; primary materials represent the principal material used whilst accent materials can be used on particular features (e.g. on corners or as window surrounds). The palette of accent materials serving to reinforce the distinctiveness and legibility throughout the coding area without detriment to the distinctiveness of the MKWEA as a whole. Throughout the design code area there should be variety in the accent materials.

MANDATORY EXTERNAL MATERIALS PALETTE



14. Street Furniture

14.1. Indicative Street Furniture

14.1.1. The diagram left provides an indication of the types of street furniture and lighting to be used within the development and the general approach to the application of street furniture which allows for formality or informality depending on the type of location. Mandatory Principles outlined above should be referred to in the first instance to determine the character of a particular location on the development before street furniture is selected. Any non-typical street furniture would need to be agreed with the relevant highways authority.

14.1.2. Whitehouse South Square provides an opportunity for a public art project.



14.1.3. Seating located throughout the scheme will respond to place, for example, Whitehouse South will be more formal, whereas the Green Corridors will be progressively informal. As a result there will need to be variety in material, form and character.

1.1.1. A simple selection of bins and bollards located throughout the scheme will subtly respond to place, character and local context, addressing formality and character in a coordinated /similar manner with/ to seating.

1.1.2. Signage/ wayfinding should be incorporated into the design, where appropriate as part of a site wide strategy to ensure simplicity and coordination.

1.1.3. Various lighting methods (Street lights, tree uplights, path lighting) will be incorporated around the scheme to create a cohesive lighting strategy that enhances street hierarchy, provides visual interest and promotes safety.

1.1.4. High quality themed play equipment will be developed within each play area to create a cohesive set of play spaces that challenges the needs of children with varying ages/ability. Timber equipment can provide a natural approach and allow play areas to sit within the landscape.

1.1.5. Incidental play and creative land modelling can be located where appropriate to encourage informal play and social opportunities/ interaction.

SECTION D /
COMPLIANCE CHECKLIST

15. Compliance Checklist

15.1. Overview

15.1.1. The Compliance Checklist below sets out the key requirements of the code in tabular form and provides a useful cross reference tool to the fundamental components of the Design Code. Full compliance with the code is expected, and a full supporting justification will be required for any deviation from it.

15.1.2. Designers of Reserved Matters Applications are required to take full account of the provisions of the Design Code prior to formulating any detailed proposals.

15.1.3. The compliance checklist should be used as an aide memoir throughout the design process as an iterative tool. This must not be restricted to two-dimensional layout but be designed holistically including landscape, built form and materials. The designer must complete the compliance checklist, fully explaining and justifying any non-compliant aspects of the proposal.

15.1.4. A completed compliance checklist must be submitted as part of the Reserved Matters Application. In determining Reserved Matters Applications, MKC should establish that proposals are wholly compliant with both national and local policy and guidance and the Design Code unless non-compliance can be fully-explained, justified and acceptable in all respects.

MKWEA WHITEHOUSE SOUTH COMPLIANCE CHECKLIST

Do proposals comply with the mandatory written design principles of the design guide, in respect of:

Design Code Ref:

Compliant?
Yes/No

Comment/Action

MOVEMENT HIERARCHY

City Street

Link Street

Residential Street

Edge/Green Street

Community Street

Mews Street

KEY SPACES

Whitehouse South Neighbourhood Green

Green Corridors

KEY LANDMARKS & FRONTAGES

Whitehouse South Green Key Frontage

Key Corners

Vista Termination



Define.

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